

ZONING COMMITTEE STAFF REPORT

FILE # 17-206-385

1. **APPLICANT:** Jon Schwartzman - MCR Property Holdings LLC **HEARING DATE:** 1/04/2018
 2. **TYPE OF APPLICATION:** Site Plan Review
 3. **LOCATION:** 1973 - 1977 Marshall Ave, at the NE corner of Moore St. (To be combined as 1975 Marshall Avenue)
 4. **PIN & LEGAL DESCRIPTION:** 332923330021; 332923330022 (Merriams Rearr of Blks 24 29 W 1o Ft of Lot 8 And All of Lot 7 Blk 27, and Merriams Rearr of Blks 24 29 E 4o Ft of Lot 8 Blk 27)
 5. **PLANNING DISTRICT:** 13 – Union Park District Council **PRESENT ZONING:** RM2
 6. **ZONING CODE REFERENCE:** §61.402(c) *Site plan review and approval.*
 7. **STAFF REPORT DATE:** 12/28/2017 **BY:** Tia Anderson
 8. **DATE RECEIVED:** 10/18/2017 **DEADLINE FOR ACTION:** 2/15/2018 (Extension Letter sent)
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- A. **PURPOSE:** Site Plan Review for a new five-story, 16 unit apartment building with 30 structured parking spaces.
- B. **PARCEL SIZE:** 14,171 square feet, plus 998 square feet for half the alley (approx. 0.35 acres)
- C. **EXISTING LAND USE:** 1973 Marshall – one-family dwelling, 1977 Marshall – duplex
- D. **SURROUNDING LAND USE:**
North: One-, two- and multiple-family residential, institutional (RT1, RM2)
East: One-, two- and multiple-family residential (RM2)
South: One-, two- and three-family residential, and institutional (RM2)
West: One-, two- and multiple-family residential, and congregate living (RM2)

- E. **ZONING CODE CITATION:**
 - §61.402(c) - Findings for site plan review.

F. **HISTORY/DISCUSSION:**

A development moratorium was approved by the Saint Paul City Council on October 18, 2017, on Marshall Avenue between Wheeler and Wilder Streets within the West Marshall Study Area, which was established for purposes of a zoning study. This project is within the moratorium area, however, a complete Site Plan application was submitted prior to the City Council public hearing on October 18, 2017.

The property is currently developed with a single-family dwelling and duplex. The applicant is proposing a new five-story, 16 unit apartment building with structured parking (22 in a below grade parking level accessed from Moore St. and 8 on the ground floor accessed from a driveway off the alley). The originally submitted Site Plan included two accessory garages, which have since been eliminated. (See attached plans)

A Site Plan Review Committee meeting was held for the proposed project on 11/7/2017 and

subsequent staff feedback provided on 12/20/2017 (see attached Committee Reports). Site Plan Review is a function delegated by the Saint Paul Planning Commission to City staff, however, a Site Plan may be referred to Planning Commission for public hearing and decision.

G. DISTRICT COUNCIL RECOMMENDATION:

The Union Park District Council (DC 13) did not provide a recommendation to approve or deny the Site Plan. The District Council hosted community meetings on Oct. 30 and Dec. 18, 2017. The District Council sent a letter on November 6, 2017 summarizing community feedback (see attached).

H. FINDINGS: Section 62.108(c) of the Zoning Code says that in “order to approve the site plan, the planning commission shall consider and find that the site plan is consistent with” the findings listed below.

1. *The city's adopted comprehensive plan and development or project plans for sub-areas of the city.*

The site plan meets this finding. The proposed development is consistent with the *Saint Paul Comprehensive Plan*. The future land use map of the *Saint Paul Comprehensive Plan* (2010) identifies Marshall Avenue as a Residential Corridor. Residential Corridors are described as segments of street corridors that run through Established Neighborhoods and are predominately characterized by medium density uses (4 to 30 units per acre). The plan adds that some portions of Residential Corridors could support additional housing.

The proposed development is for 16 residential units on a property of approximately 0.35 acres. When including the density bonus for 30 structured parking spaces, the lot area for density purposes is .55 acres and the residential density is approximately 29 units per acre.

Applicable Land Use Strategies include:

- LU 1.1 – Guide the development of housing in Established Neighborhoods, commercial areas within Established Neighborhoods, and in Residential Corridors.
- LU 1.9 – Encourage the development of medium density multi-family housing along Residential Corridors.

Figure H-K of the Housing Chapter identifies this portion of Marshall Avenue as an opportunity area for potential new multifamily housing. Applicable Housing Strategies include:

- H 1.1 – Increase housing choices across the city to support economically diverse neighborhoods.
- H 1.3 – Revitalize the city by developing land-efficient housing.

The proposed development is consistent with the *Union Park Community Plan* (2016), which describes the neighborhood’s goals including the preservation of desirable assets and neighborhood character as well as development to meet present and future needs. Applicable strategies include:

- LU 2 – Preserve the well-kept, traditional feel and scale of the neighborhood.
 - LU 2.3 – Ensure that new development fits within the character and scale of adjacent neighborhoods.
- H 1 – Preserve Union Park’s pedestrian-scale neighborhoods, while promoting a range of housing types and affordability to meet the needs of people at different life stages with different housing needs.

The area is a mix of residential and institutional uses. A variety of commercial uses are also located one block to the west.

2. *Applicable ordinances of the City of Saint Paul.*

The site plan meets this finding. The site plan meets the standards in the RM2 zoning district for density, setbacks, height, parking, and design per the following:

- §66.216 – Intent, RM2 medium-density multiple-family residential district.
- §66.230 – Residential District Density and dimensional standards.
- §66.232 – Maximum lot coverage.
- §66.105 – Porches and decks.
- §60.203 – B. Building height.
- §63.207 – Parking requirements by use.
- §63.313 – Visual screening.
- §63.110 – Building design standards.

Density: The site plan meets the minimum lot area per residential unit of 1,500 square feet in a RM2 district. The parcel area is 14,171 sf + half the alley of 998 sf + lot area density bonus for structured parking of 9,000 sf = 24,169 sf / 16 units = 1,510 sf/unit.

- One-half the width of a dedicated public alley adjoining the lot shall be considered as part of the lot, for the purpose of applying lot area and density requirements.
- The lot area figure may be increased by three hundred (300) square feet for each parking space (up to two parking spaces per unit) within a multiple-family structure or otherwise completely underground.

Lot Coverage: The site plan meets the maximum 35% lot coverage in a residential district. The building area, including balconies, is 5,239 sf / 15,169 sf lot area = 34.5% lot coverage.

- One-half the width of a dedicated public alley adjoining the lot shall be considered as part of the lot, for the purpose of applying lot area and density requirements.
- An open, uncovered porch or deck shall be subject to setback and lot coverage requirements.

Setbacks: The site plan meets the minimum setbacks. In RM2 for these properties, the minimum front setback is 29' 6" based on the average setback for the block (25' setback is standard). The minimum rear setback requirement is 25'. The minimum side setback requirement is 9'. The site plan proposes a 9' sideyard setback on the west (Moore St side) and 28' sideyard setback on the east adjoining the residential property. Required off-street surface parking setback is 4' from any lot line. There is no minimum setback for driveways.

Height: The site plan meets the maximum building height of 5 stories and 50' in a RM2 district. Building height is measured from the established grade to the top of the roof deck. Where a building is located on sloping terrain (e.g., Moore St elevation), the height may be measured from the average ground level of the grade at the building wall. Due to differences in grade, a weighted average of the existing established grade is calculated at 899.7. The proposed first floor elevation is 902.0 and the top of the roof deck is measured at 949.7.

- The existing grade of the property shall not be raised around a new building or foundation in order to comply with the height requirements of this code.
- The height limitations of this code shall not apply to mechanical service stacks, tanks, ventilation equipment, and similar equipment.

Parking: The site plan includes 30 structured off-street vehicle parking spaces, which meets the required parking (3 – 3 bedroom units x 1.5 spaces = 4.5 spaces, 13 – 4 bedroom units x 2 spaces = 26 spaces. 30.5 = 30 spaces per Zoning Code). In addition there is bicycle parking proposed in the garage areas. 2 bicycle spaces are required.

Screening: Off-street parking facilities that adjoin a residential use or zoning district, shall provide a visual screen. The site plan includes a 4.5' – 6.5' fence along the east property line to visually separate the driveway from the adjoining property.

Design standards: Staff has reviewed the site plan in relation to the building design standards and found that all relevant standards are met.

3. *Preservation of unique geologic, geographic or historically significant characteristics of the city and environmentally sensitive areas.*

The site plan meets this finding. The proposed use is typical of the intent of a RM2 medium-density multiple-family residential Zoning District, which is intended to provide for comprehensive development of multiple-family uses and a balance of population concentration near major thoroughfares, transit, and related facilities. The property is not designated as having historical or environmental significance nor inventoried by Heritage Preservation.

4. *Protection of adjacent and neighboring properties through reasonable provision for such matters as surface water drainage, sound and sight buffers, preservation of views, light and air, and those aspects of design which may have substantial effects on neighboring land uses.*

The site plan meets this finding. The effect of this specific proposed building on neighboring properties is reasonable. Specific to the findings:

- The stormwater system shall meet City standards for run-off rate control. The system will consist of pipes buried below grade located on the south side of the building in the landscaped setback area. In addition, roof drainage shall meet plumbing code requirements.
- The building is oriented to the southwest corner of the property towards the street corner. The building's proposed setbacks meet or exceed the zoning requirement, providing a reasonable distance from the abutting homes.
- The setback areas to the south and east will be landscaped. Existing boulevard trees will be protected where possible and new boulevard trees will be planted as required.
- Off-street parking will be provided as two levels of structured parking: one below grade accessed from Moore St., the other at ground level accessed via a driveway off the alley. Refuse and recycling will be wheeled from the garage level to the curb for collection.
- The proposed site plan shall adhere to §63.110 – Building design standards, including delineation of a primary entrance, direct pedestrian connection to the street, building materials, minimum window and door openings, and reducing visual impact of rooftop equipment.
- A photometric plan was provided that meets zoning requirements to reduce glare, arranged as to reflect lights away from all adjacent residences and not to interfere with the vision of persons on adjacent highways or adjacent property.

The applicant submitted a shadow study, though one is not required. The study compares the existing buildings, a hypothetical 3-story building with a 9' east sideyard setback, and the proposed structure. Based on the study it appears the most significant shadows on abutting properties are late afternoon and evening near sunset (see attached).

Through the review process, the project has made site plan changes to improve the relationship of the development to the abutting residential properties, decreasing the number of balconies on the west façade, removing all balconies on the east façade, and eliminating the accessory garages.

5. *The arrangement of buildings, uses and facilities of the proposed development in order to assure abutting property and/or its occupants will not be unreasonably affected.*

The site plan meets this finding. This segment of Marshall Ave includes a variety of housing options including multiple-family residential as well as institutional uses. As a buffer to abutting residential properties, the site plan proposes a 29' 6" front setback, 25' rear setback plus the alley, 9' sideyard setback on the west (Moore St side), and 28' sideyard setback on the east. Balconies are situated on the Moore St side of the building rather than facing the interior lot lines.

Required off-street parking will be provided within the building. Landscaping and fencing along the east property line will visually separate the building and driveway from the adjoining property. Landscaped retaining walls will be provided at the southwest corner of the property. The effect of this specific proposed building on neighboring properties is reasonable.

6. *Creation of energy-conserving design through landscaping and location, orientation and elevation of structures.*

The site plan meets this finding. A multifamily building is inherently more energy-conserving because it has fewer exterior walls and roof per dwelling unit than low density housing. The building is oriented to the southwest corner of the site and exceeds the minimum amount of glazing on all sides, allowing each unit to gain solar heat.

The proposed development is located on a minor arterial street with good public transit, a bus stop at the property, and dedicated bicycle lane, making it conducive to walking, biking, and using public transit rather than driving.

7. *Safety and convenience of both vehicular and pedestrian traffic both within the site and in relation to access streets, including traffic circulation features, the locations and design of entrances and exits and parking areas within the site.*

The site plan meets this finding. The site plan includes structured parking: 22 spaces in a below grade parking level accessed from Moore St. and 8 spaces on the ground floor accessed from a driveway off the alley. Refuse and recycling will be stored in the garage level and wheeled to the curb for collection.

The number of residents and visitors to the property did not warrant a traffic memo or study by Public Works Transportation and Safety. The City Traffic Engineer has accepted the site plan in concept. A condition of Site Plan approval should include vehicle turning templates for the parking areas be approved by the Public Works Transportation Planning and Safety Division.

Ramsey County had no comments on the plan as submitted. Metro Transit requires replacement of the existing bus stop at the corner of Marshall Ave and Moore St. Metro Transit and the developer have acknowledged that the landscaped retaining wall near the bus stop will likely be used for seating by transit users.

8. *The satisfactory availability and capacity of storm and sanitary sewers, including solutions to any drainage problems in the area of the development.*

The site plan meets this finding. The utility connections are shown on Sheet C4. Water, Sanitary and Storm sewer services are available in Marshall Avenue.

Stormwater from the site would be piped to an underground detention system located on the south of the property; stormwater would go out to the public storm sewer in Marshall at a controlled rate that meets City standards. Drainage maps and HydroCAD modeling to meet the City's stormwater run-off rate control standards were completed. A condition for site plan approval should include that technical details for the stormwater management solution be approved by the Public Works Sewer Division.

9. *Sufficient landscaping, fences, walls and parking necessary to meet the above objectives.*

The site plan meets this finding. A condition of Site Plan approval should include a final landscape plan be approved by the City Forester. The setback areas to the south and east will be landscaped and used for stormwater management. A 4.5' – 6.5' fence will be provided along the east property line to visually separate the driveway from the adjoining property. Landscaped retaining walls will be provided at the southwest corner of the property. Existing boulevard trees will be protected where possible and new boulevard trees planted as required.

A minimum of 30 off-street parking spaces are required based on the residential mix of three 3-bedroom and thirteen 4-bedroom dwelling units. 30 structured off-street parking spaces are proposed, 22 in the below grade parking level and 8 (including 2 ADA spaces) in the enclosed ground floor parking area. Accessory parking facilities may designate up to 50 percent of the spaces for compact cars. Two bicycle parking spaces are required based on the number of off-street parking spaces provided; however, the project is proposing additional bicycle parking within the two off-street parking levels.

10. *Site accessibility in accordance with the provisions of the Americans with Disabilities Act (ADA), including parking spaces, passenger loading zones and accessible routes.*

The site plan meets this finding. The plan proposes 2 accessible parking spaces to meet the ADA standards required for lots of 26 - 50 parking spaces. Required accessible entrances and routes shall be provided per accessibility code. The public sidewalks have accessible crossings.

11. *Provision for erosion and sediment control as specified in the ``Ramsey Erosion Sediment and Control Handbook.''*

The site plan meets this finding. The site plan includes an erosion and sediment control plan that meets this standard.

I. STAFF RECOMMENDATION:

Based on the findings above, staff recommends approval of the site plan to allow a five-story, 16 unit multi-family apartment building with structured parking at 1973 - 1977 Marshall Ave., subject to the following conditions:

1. The building shall be substantially as presented and illustrated to the Planning Commission, using materials that are the same or better.
2. Final approval by the Public Works Transportation Planning and Safety Division.
3. Final approval by the City Forester.
4. Final approval by the Public Works Sewer Division.



City of Saint Paul Department of Safety & Inspections, 375 Jackson Street, Suite 220, Saint Paul MN 55101

SITE PLAN REVIEW APPLICATION

Date Application Received:

RECEIVED

OCT 18 2017

By: City of St Paul DSI

Staff Use Only

SPR File # 17-206385

Application Fee \$ 735

Staff Meeting Date: Nov 7, 2017

City Agent: [Signature]

Project Name: MARSHALL & MOORE APARTMENTS	
Site Address: 1973/ 1977 MARSHALL AVENUE	Property Identification Number: 33.29.23.33.0022/ 33.29.23.33.0021
Project Description: FIVE STORY - 16 UNIT APARTMENT BUILDING WITH ONE-STORY OF UNDERGROUND PARKING BELOW. TWO ON GRADE PARKING GARAGES AND ON-SITE SURFACE PARKING	

Provide (5) five Paper Copies 11x17 and an electronic PDF version (11x17 print format) of the complete Site Plan package including **certificate of survey, civil site plan, exterior architectural plan, and landscape plan.**

Project Summary

Est. Project Cost: \$ (exclusive of land value) \$4,000,000	Est. Construction Start JUNE 1, 2018	Proposed Land Use:
Parcel Area [sq. ft.] 14,171 SQ FT	Disturbed Area [sq. ft.] 14,171 SQ FT	<input checked="" type="checkbox"/> Residential <input type="checkbox"/> Institutional <input type="checkbox"/> Parking <input type="checkbox"/> Commercial <input type="checkbox"/> Industrial Only <input type="checkbox"/> Mixed-Use <input type="checkbox"/> Other
Floor Area Ratio 1.63	Building Gross Floor Area 4,960 SQ FT	# Off-Street Parking Spaces 32
<input type="checkbox"/> Historic District/Property N/A	<input type="checkbox"/> Flood Plain Property N/A	<input type="checkbox"/> Steep Slope (>12%) N/A

Residential Project Details

# Residential Units 16	# Affordable 0	% AMI for Affordable N/A
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Applicant Information [Name, company, address, phone, e-mail]

Developer or Property Owner MCR PROPERTY HOLDINGS, LLC JON SCHWARTZMAN 10921 EXCELSIOR BLVD #106 HOPKINS, MN 55343 (612) 282-3615 JONSCI@MSN.COM	Project Contact [PM, architect] POPE ARCHITECTS, INC. PAUL HOLMES 1295 BANDANA BLVD N SUITE 200 ST. PAUL, MN 55108-2735 (651) 789-1582 PHOLMES@POPEARCH.COM	Construction Contact TBD
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Signature

Date

10/18/17

Staff Use Only

Zoning District RM 2	Overlay Zoning District	District Council 13
Ward 4	Watershed District	MnDOT or County 438
<input checked="" type="checkbox"/> Parkland Dedication	<input type="checkbox"/> TDMP <input type="checkbox"/> CUP Required	Previous SPR

1973 1977 MARSHALL AVENUE APARTMENTS



MCR Property Holdings, LLC
MARSHALL & MOORE APARTMENTS
SAINT PAUL, MINNESOTA

VIEW FROM SOUTHWEST

SITE PLAN REVIEW
12/28/2017
51666-17088

PROJECT TEAM:

OWNER:
MCR PROPERTY HOLDINGS, LLC
10921 EXCELSIOR BLVD SUITE 106
HOPKINS, MN 55343
(612) 282-3615
JON SCHWARTZMAN

ARCHITECT:
POPE ARCHITECTS, INC.
1295 BANDANA BLVD N SUITE 200
SAINT PAUL, MN 55108
(651) 642-9200
PAUL HOLMES

CIVIL ENGINEER:
LARSON ENGINEERING, INC.
3524 LABORE ROAD
WHITE BEAR LAKE, MN 55110
(651) 481-9120
ERIC MEYER

LIGHTING CONSULTANT:
EMANUELSON-PODAS C.E.
7705 BUSH LAKE ROAD
EDINA, MN 55439
(952) 930-0050
DANNY JOHNSON

POPE
ARCHITECTS

UNIT DATA

	3 BEDRM	4 BEDRM	TOTAL BEDS
FIRST FL -		1 UNIT	4 BEDS
SECOND FL -	1 UNIT	3 UNITS	15 BEDS
THIRD FL -	1 UNIT	3 UNITS	15 BEDS
FOURTH FL -	1 UNIT	3 UNITS	15 BEDS
FIFTH FL -		3 UNITS	12 BEDS
TOTAL -	3 UNITS	13 UNITS	61 BEDS

PARKING DATA

PARKING REQ.		
3 - 3 BR UNITS X (1.5 CARS PER UNIT) =		4.5 CARS
13- 4 BR UNITS X (2 CARS PER UNIT) =		26 CARS
TOTAL		30 CARS

PARKING PROVIDED		
BELOW GRADE PARKING - (ENCLOSED)	9 STANDARD CARS	13 COMPACT CARS
FIRST FLOOR PARKING - (ENCLOSED)	4 STANDARD CARS	2 HANDICAPPED CARS
	2 COMPACT CARS	
TOTAL	30 CARS	
BICYCLE PARKING	9 BIKES	

LOT AREA DATA

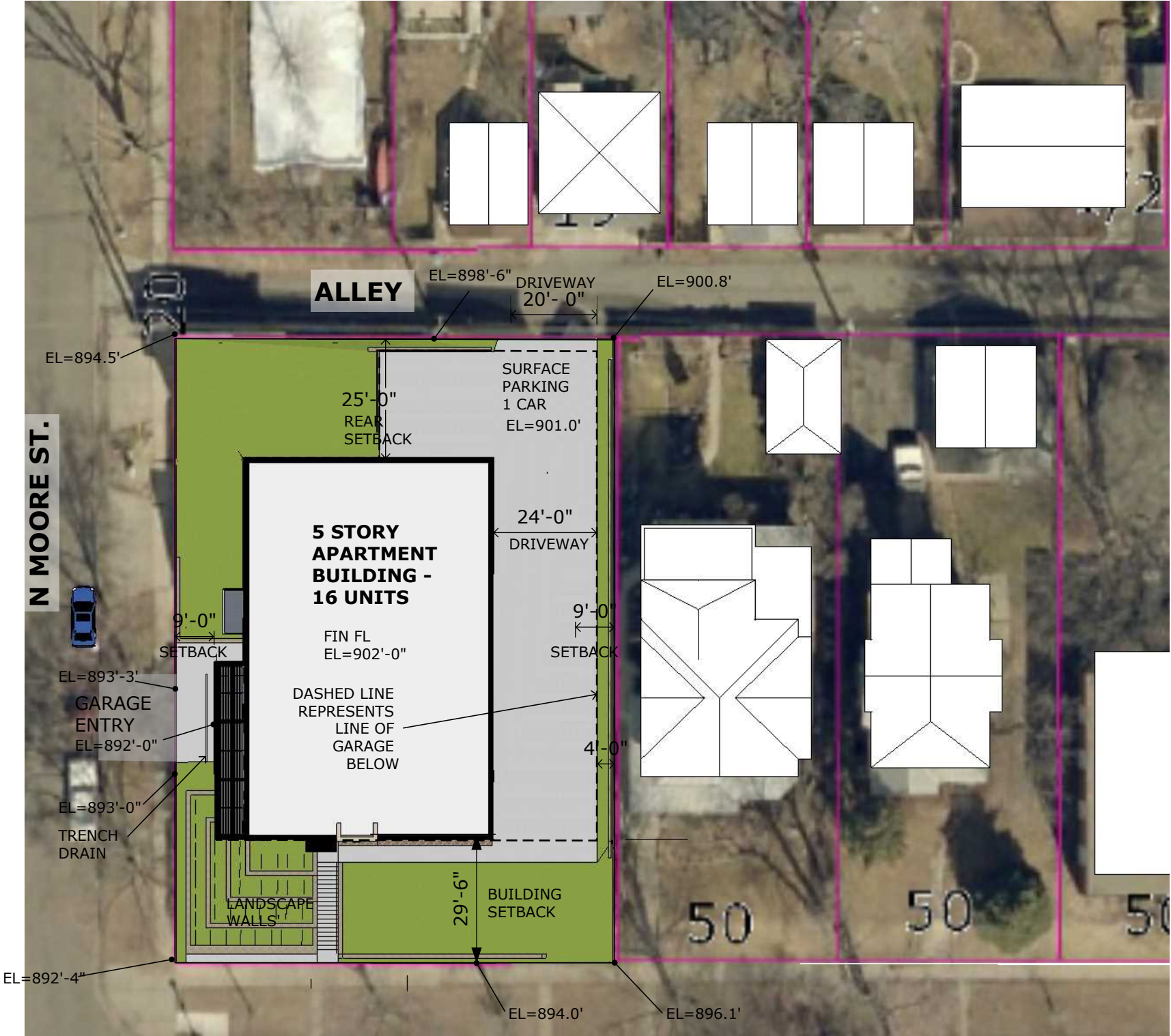
16 units - @ 1,500 SF =	TOTAL	24,000 SF
LOT DIMENSIONS FR AREA =	99.8 X 141.9	14,171 SF
1/2 ALLEY=	10 x 99.8	998 SF
30 ENCLOSED STALLS X 300 =	BONUS	9,000 SF
		24,169 SF

LOT COVERAGE:		
LOT AREA:	14,171 SF	
1/2 ALLEY:	998 SF	
	15,169 SF	= 5,285 SF @35%

BLDG AREA:	5,190 SF	
WEST TERRACE AREA:	INC ABOVE	
WEST BALCONY AREA:	49 SF	
	5,239 SF	COVERAGE RATIO: = less than 35%

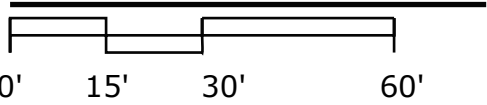
MCR Property Holdings, LLC
MARSHALL & MOORE APARTMENTS
SAINT PAUL, MINNESOTA

SITE PLAN REVIEW
12/28/2017
51666-17088



MARSHALL AVE

SITE PLAN



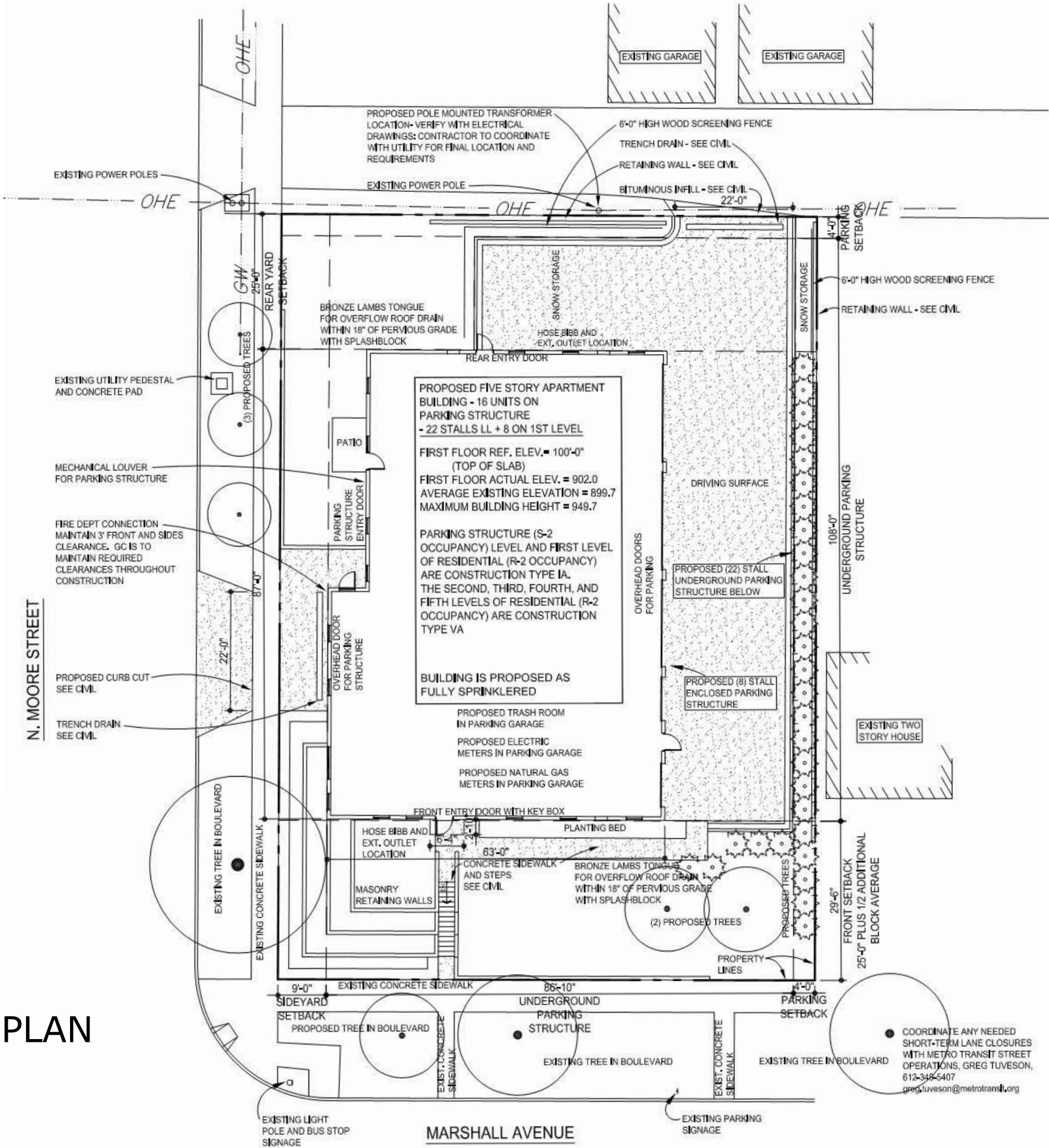
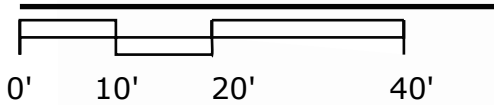


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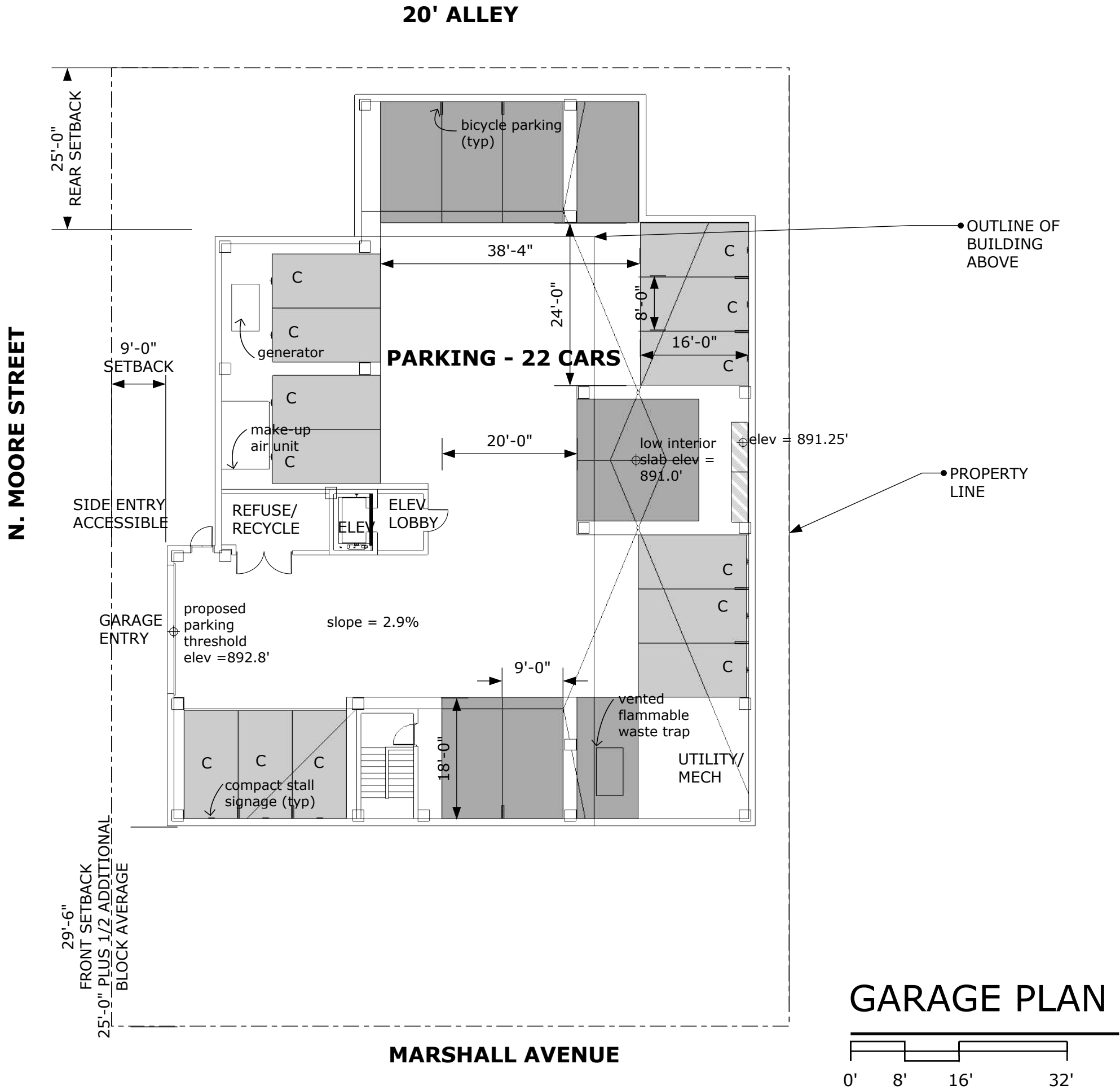


ARCHITECTURAL SITE PLAN



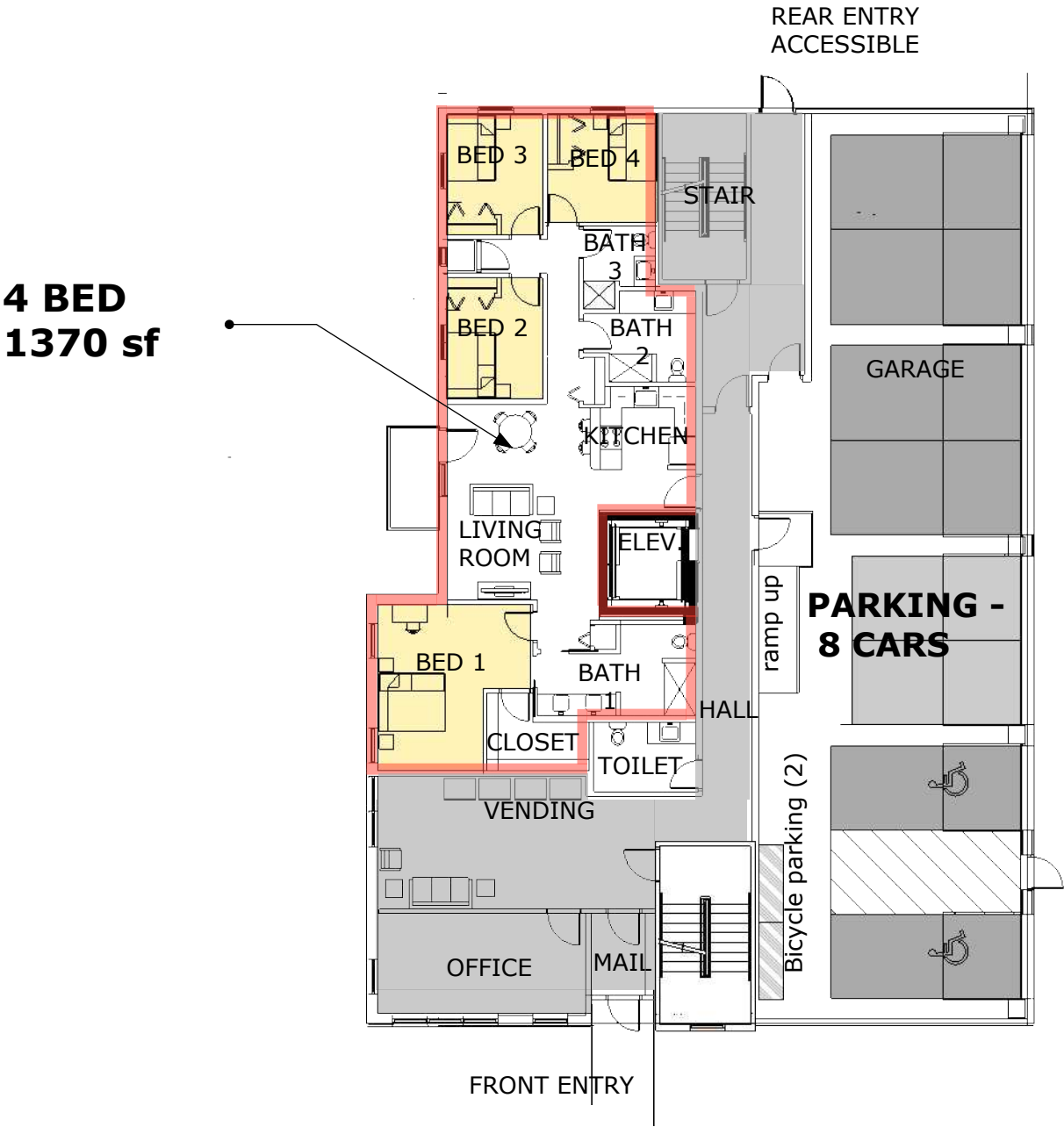
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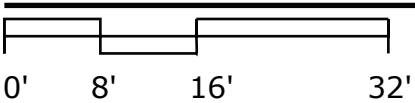


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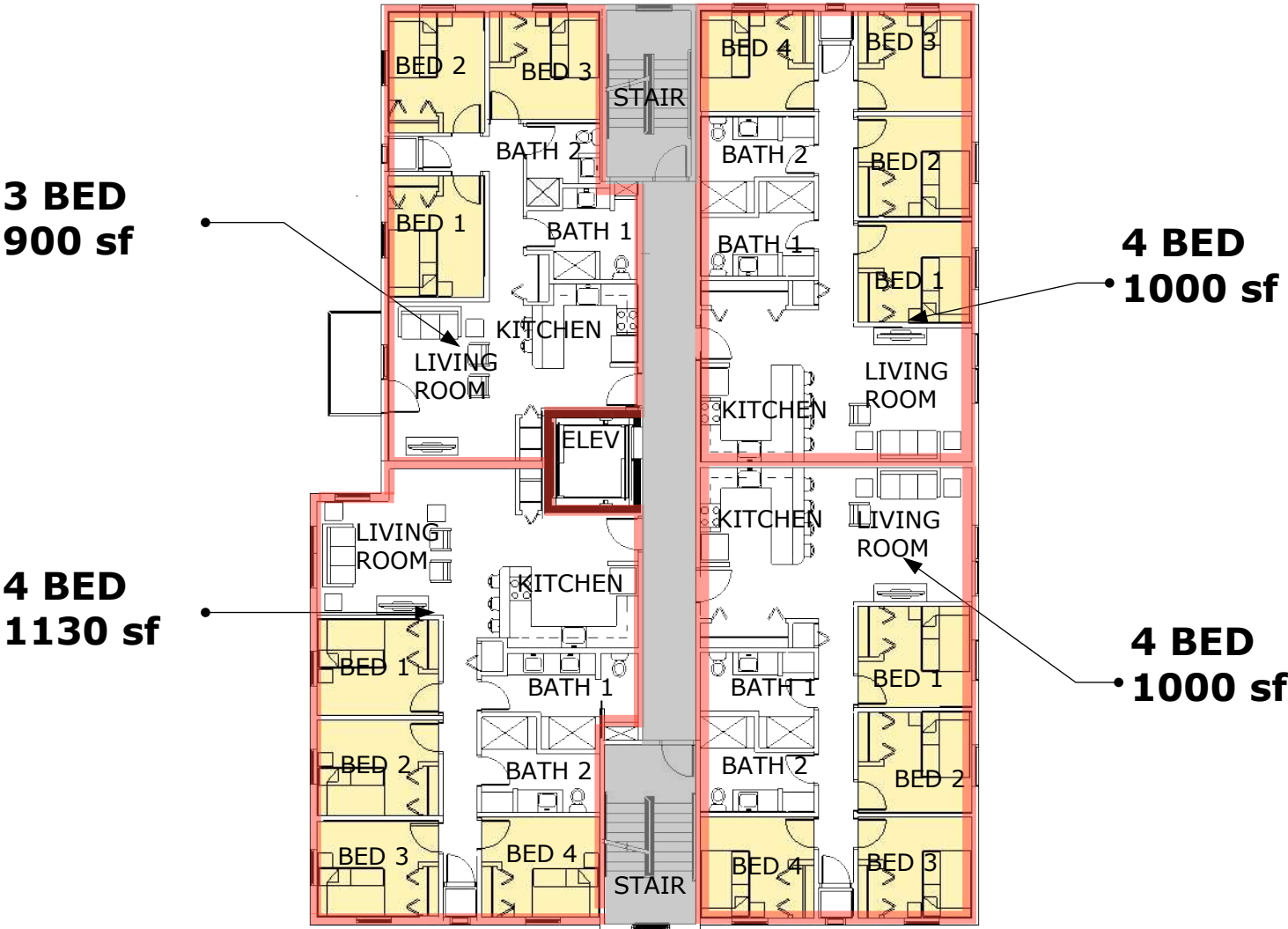


GROUND FLOOR
PLAN



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FLOOR PLAN -
2nd-4th LEVELS

A horizontal scale bar with four segments, labeled "0'", "8'", "16'", and "32'" from left to right.

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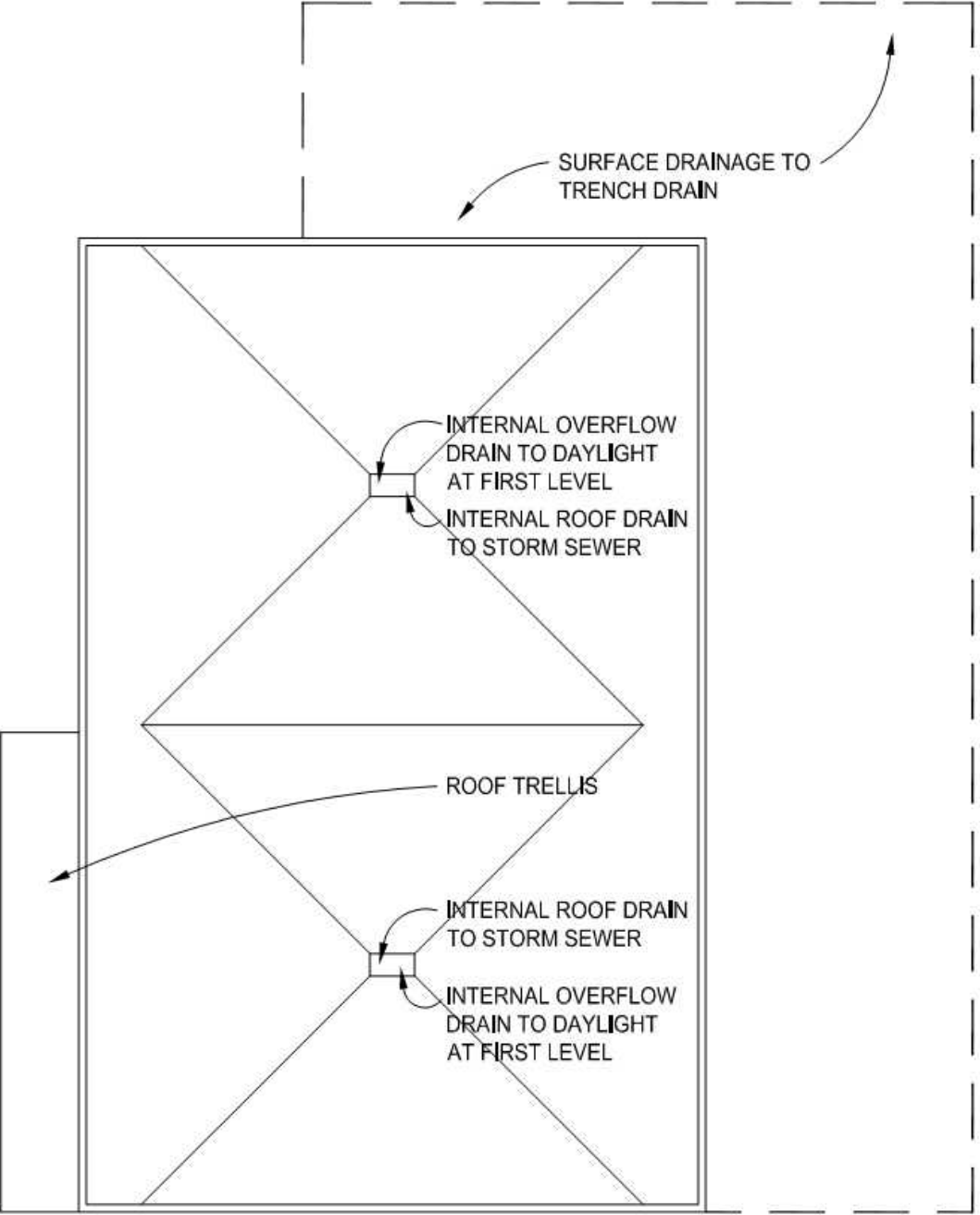


FLOOR PLAN -
FIFTH LEVEL

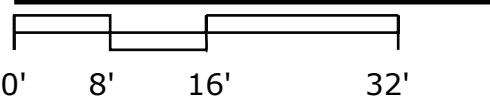
A horizontal scale bar with markings at 0', 8', 16', and 32'.

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SAINT PAUL, MINNESOTA

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ROOF PLAN





WEST ELEVATION

21.02% glass

SOUTH ELEVATION

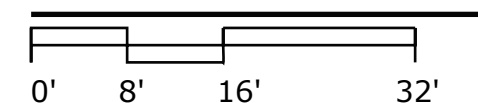
17.21% glass

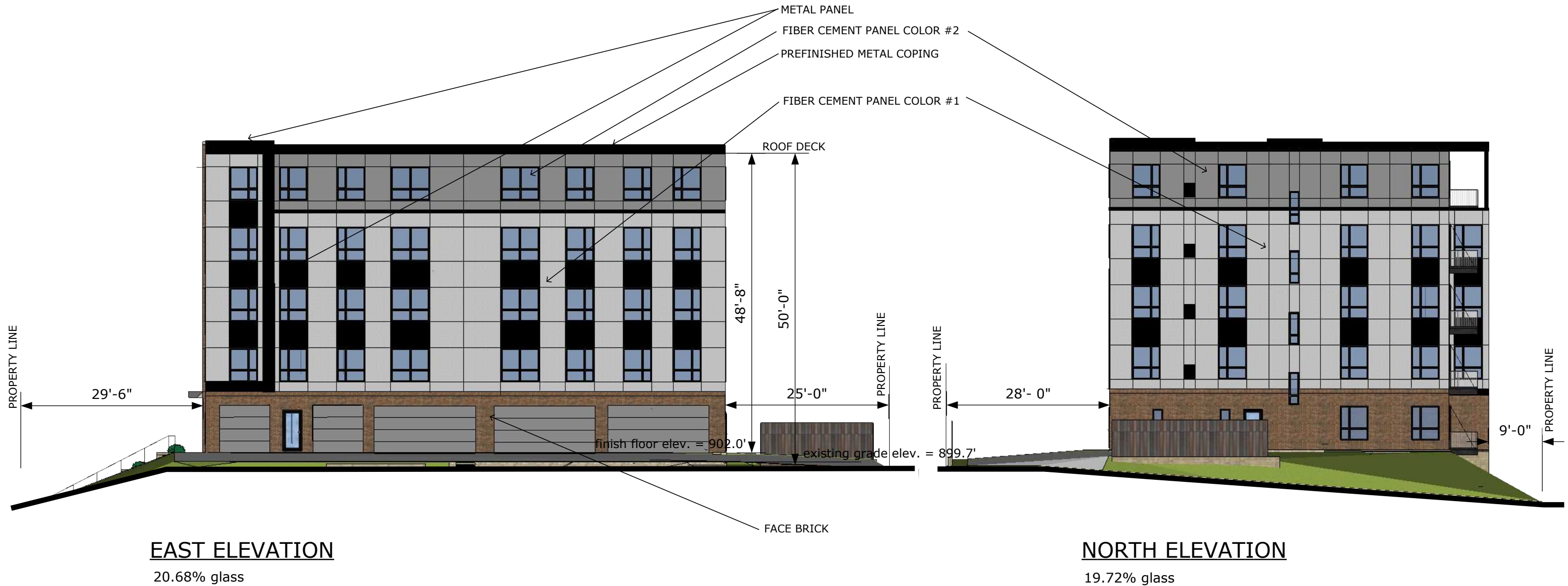
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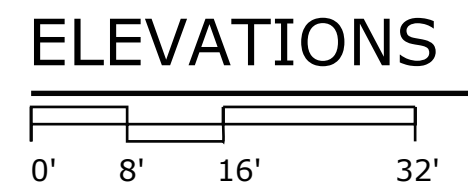
ELEVATIONS

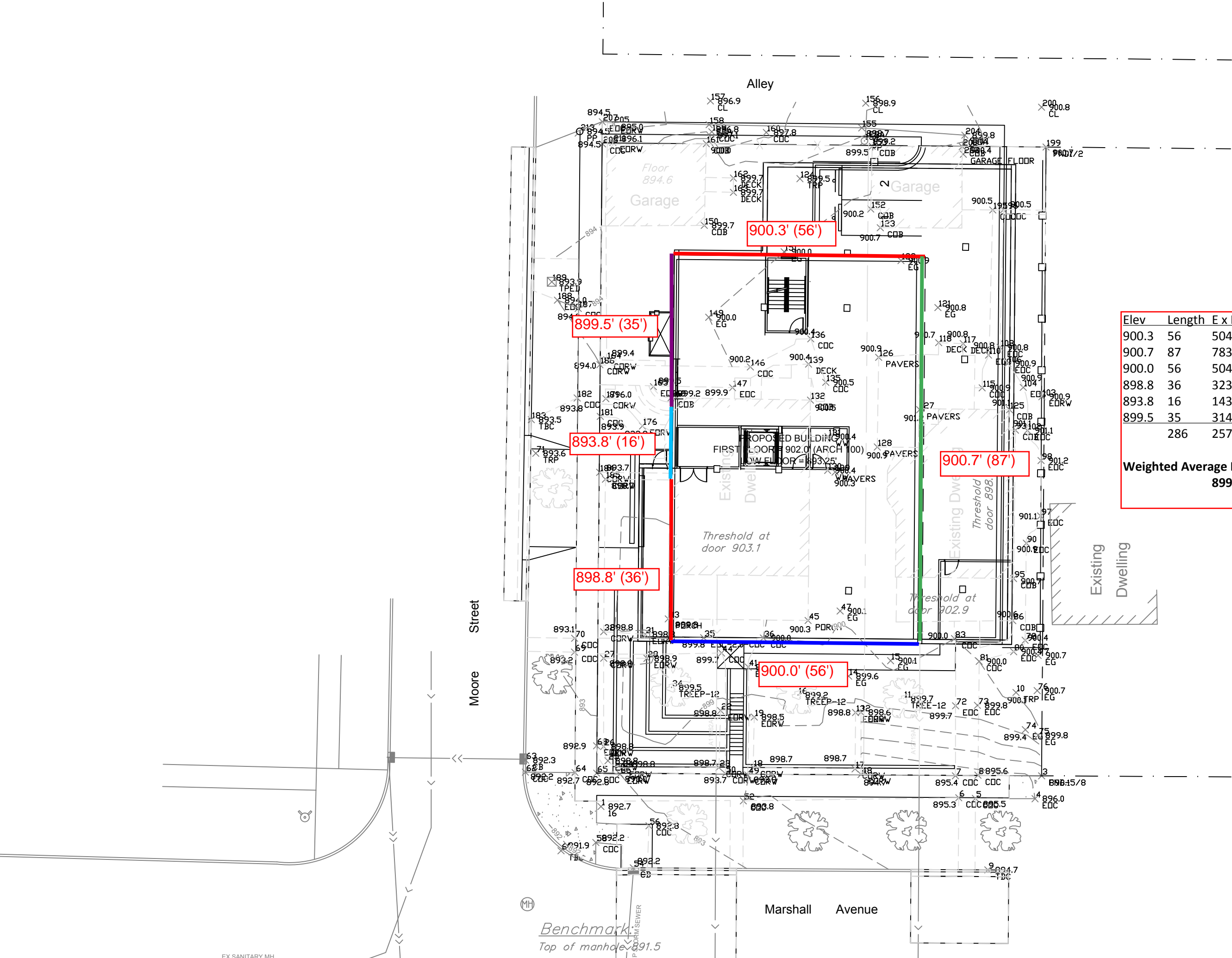




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SAINT PAUL, MINNESOTA

SITE PLAN REVIEW
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Elev	Length	E x L
900.3	56	50417
900.7	87	78361
900.0	56	50400
898.8	36	32357
893.8	16	14301
899.5	35	31483
	286	257318
Weighted Average Elevation		899.7

L O O O L O O O C R P O O O O R L O P O R C L O O O O M O R H O L L O O O O

Lot 1, Block 1, MERRIMACK RIVER O O O O O O O O O L O C O O 1, 1, 1, 1, 1 MERRIMACK
P O R O, Ramsey County, Minnesota, except the east feet thereof

L O O O L O O O C R P O O O O R L O P O R C L O O O O M O R H O L L O O O O

the east feet of Lot 1, and all of Lot 1, Block 1, MERRIMACK RIVER O O O O O O O O O L O C O O
1, 1, 1, 1, 1 MERRIMACK P O R O, Ramsey County, Minnesota

P R O P O O O O L O O O L O O O C R P O O O O R L O O C O M O O O O O O O

Lots 1 and 1, Block 1, MERRIMACK RIVER O O O O O O O O O L O C O O 1, 1, 1, 1, 1
MERRIMACK P O R O, Ramsey County, Minnesota

C O P O O O O R O O L M O O O O O O O

- the following the length and direction of boundary lines of the legal description listed above the scope of
of services does not include determining that you own, that is legal matter. Please see the
legal description in your records or consult with a competent legal counsel, if necessary, to make sure
that it is correct and that any matters of record, such as easements, that you wish to be included on the
survey are shown.
- the following the location of existing improvements are deemed necessary for the survey.
- setting survey markers or verifying existing survey markers to establish the corners of the property.
- the following elevations on the site at selected locations to give some indication of the topography of the
site are also provided for your use in determining elevations for construction on
this site. The elevations shown relate only to the benchmark provided on this survey. Use that
benchmark and at least one other reference point on the survey when determining other
elevations for use on this site or before beginning construction.
- the scope of proposed division of the property. Please refer to the proposal to see that it is that you
intend and submit to those governing agencies that have jurisdiction over their approvals, if
you wish, before making any decisions regarding the property.
- Note that all building dimensions and building tie dimensions to the property lines, are taken from the
siding and not from the building.

O O L O O O O O O O

- The utilities shown are per the results as provided to us from our client.
- At the time of the revision there were no other lines shown in the field and we were unable to get
other locations. Our client explained in an email the location of the other services. These were not
field verified and need to be field verified before using the other service location as shown on this
survey.
- An underground utility locate was not done for this site due to the short amount of time needed to get
the survey completed. It is recommended that an underground utility locate be completed for this site
if this is done we can locate and revise the survey to show these field findings.

O O O O O O R O O M O O L O O C O O O O O O O O O O

"●" denotes iron survey marker, set, unless otherwise noted.

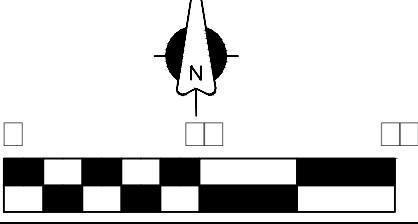
LEGEND

- CATCH BASIN
- FIRE HYDRANT
- POWER POLE
- MANHOLE
- TELEPHONE PED.
- ELEC. TRANSFORMER
- WELL
- GATE VALVE
- LIGHT POLE
- TREE
- FENCE LINE
- SANITARY SEWER LINE
- WATER LINE
- GAS LINE
- STORM DRAIN LINE
- OVERHEAD UTILITY LINE
- CONCRETE SURFACE

DATE	REVISION DESCRIPTION
11/13/17	ADD UTILITIES AND ROAD WIDTHS

DRAWING ORIENTATION & SCALE

SCALE - 1" = 20'



CLIENT NAME / JOB ADDRESS

MCR PROPERTY HOLDINGS, LLC

1973 & 1977 MARSHALL AVENUE
ST. PAUL, MN

Advance
Surveying & Engineering, Co.

17917 Highway 7
Minnetonka, Minnesota 55345
Phone (952) 474-7914
Website: www.advsur.com

I HEREBY CERTIFY THAT THIS PLAN, SURVEY OR REPORT
WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION
AND THAT I AM A DULY REGISTERED LAND SURVEYOR
UNDER THE LAWS OF THE STATE OF MINNESOTA.

Thomas M. Bloom
42379
LICENSE NO.
JUNE 14, 2017
DATE

DATE SURVEYED: JUNE 12, 2017
DATE DRAFTED: JUNE 14, 2017

SHEET TITLE
EXISTING CONDITIONS
SURVEY

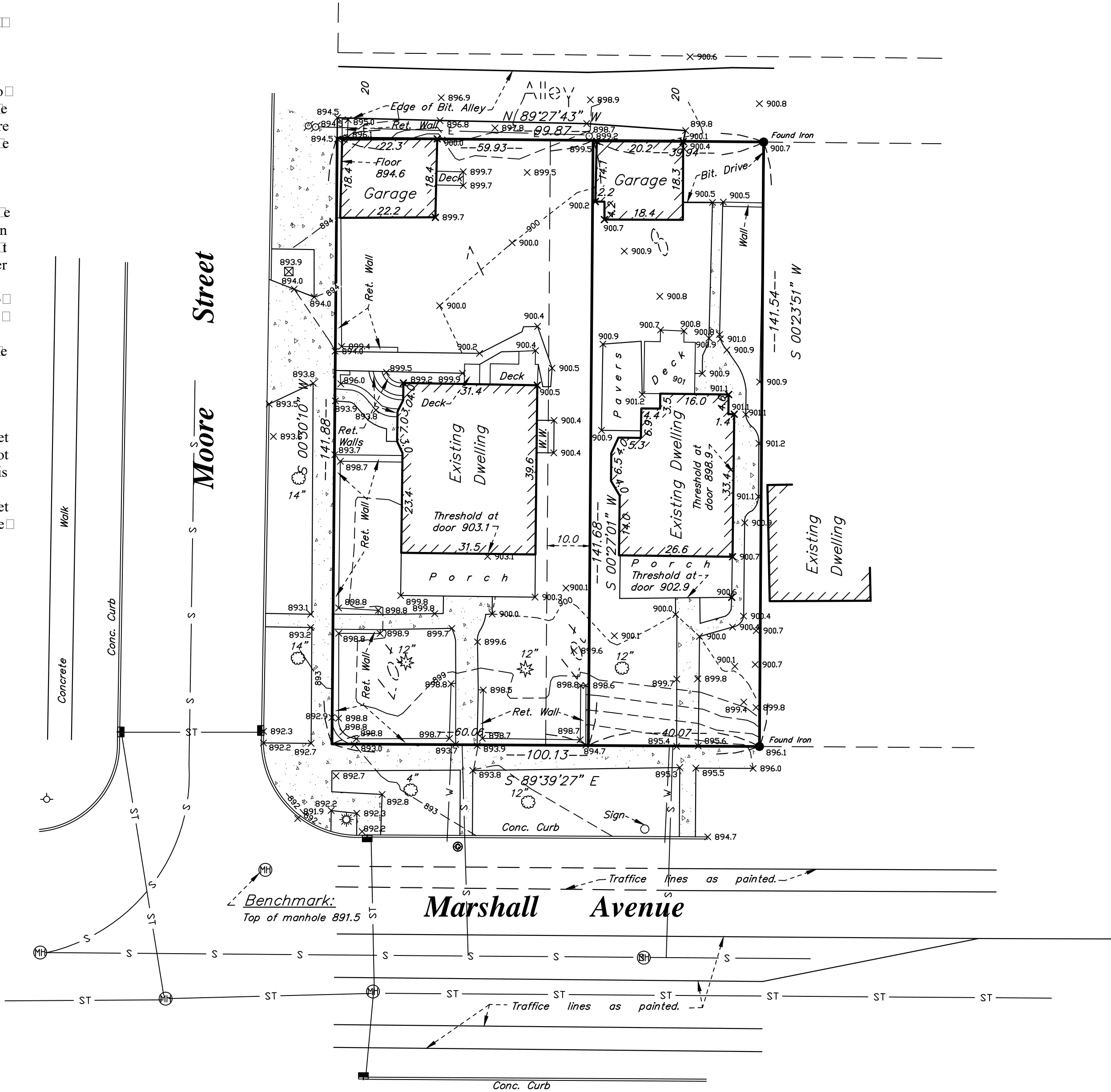
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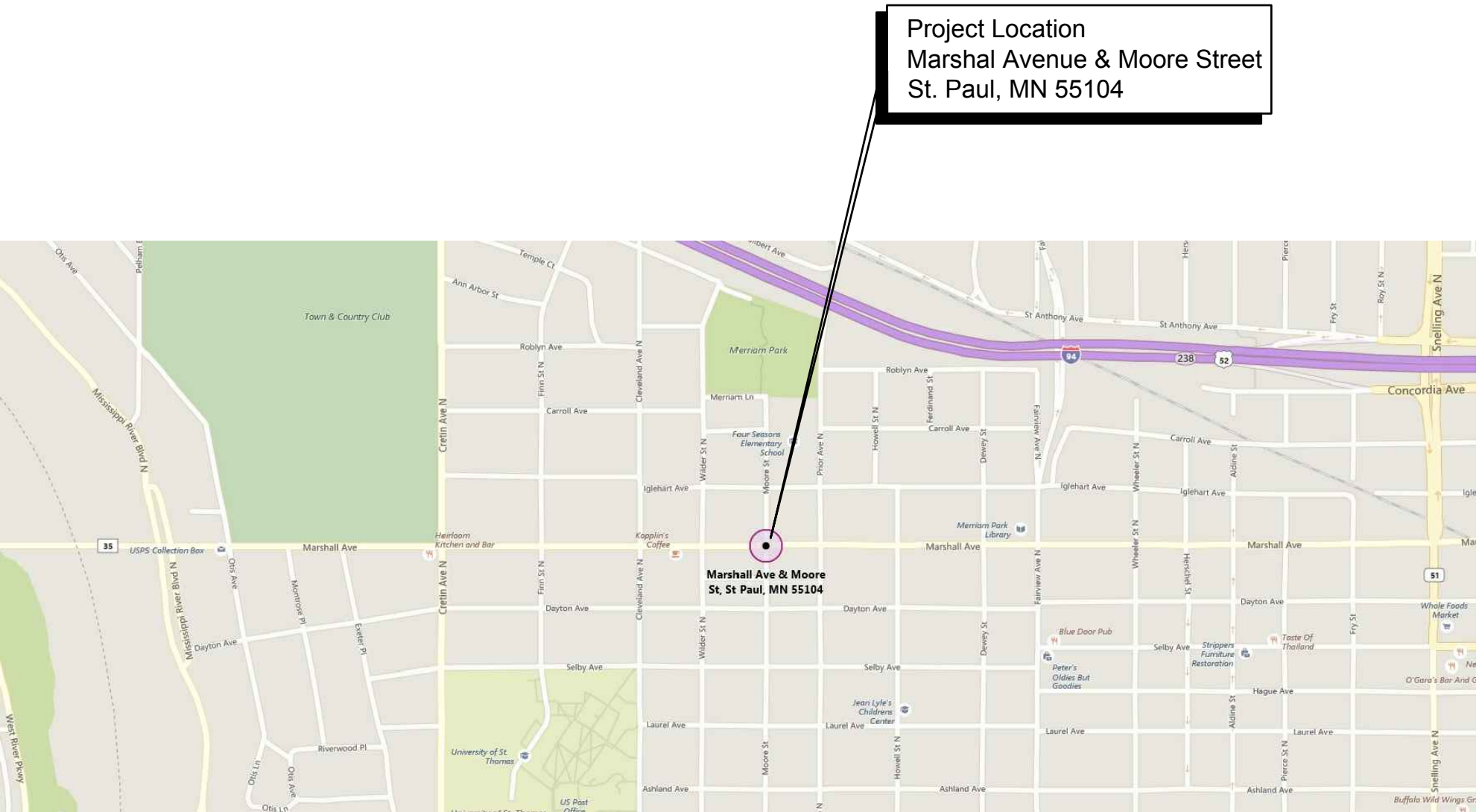
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S1

SHEET 1 OF 1



VICINITY MAP



GENERAL PLAN NOTES

TRAFFIC:
INSPECTION CONTACT: The developer shall contact the Right of Way inspection Dick Rohland, (651) 485-1688 (one week prior to beginning work) to discuss traffic control, pedestrian safety and coordination of all work in the public right of way. Note: If a one week notice is not provided to the City, any resulting delays shall be the sole responsibility of the Contractor. As part of the ROW permitting process, two weeks before any work begins that impacts the ROW in any way the developer shall provide to the ROW Inspector the name and contact information of the Construction Project Manager or Construction Project Superintendent. If this information is not provided there may be a delay in obtaining permits for the work in the ROW. Said delays will be the sole responsibility of the developer.

SAFE WORK SITE REQUIREMENTS: The Contractor shall provide a continuous, accessible and safe pedestrian walkway that meets ADA and MN MUTCD standards if working in a sidewalk area, and traffic control per MN MUTCD requirements for work in the public right of way.

NO PRIVATE FACILITIES IN THE RIGHT OF WAY: The developer is strictly prohibited from installing private electrical wiring, conduit, receptacles and/or lighting in the City's Right of Way. This includes stubbing conduit or cable into the public right of way to accommodate utility feeds to the site. Coordinate with each utility prior to construction to determine feed points into the property. Utilities are responsible for securing excavation permits to run their service into a site, and (where required) submitting plans for review by the Public Works Utility Review Committee. The contractor shall contact Don Bjorkman, Street Lighting Division of the Department of Public Works (612-266-9780).

The Contractor shall contact Don Bjorkman, General Foreman, Lighting - Signal Maintenance, (651-266-9780), if removal or relocation of existing facilities is required or in the event of damage to the lighting or signal utilities. The Contractor shall assume responsibility (and related costs) for any damage or relocations.

Access to signal controller and lighting cabinets must be maintained at all times. If fencing is required for a job site, a key or other means of access must be provided to the City of St. Paul's Traffic Operations Department. Contact Don Bjorkman, General Foreman Signals and Lighting at 651.266.9780 for more information.

ROADWAY RESTORATION policy, restoration on roadway surfaces less than 5 years old will require full width mill and overlay or additional degradation fees. Degradation fees are determined by contacting the Right of Way Service Desk at (651) 266-6151. Pavement restoration shall be completed by the St. Paul Public Works Street Maintenance Division. All related costs are the responsibility of the developer/contractor. Contact Street Maintenance at (651) 266-9700 for estimate of costs for pavement restoration.

ENCROACHMENTS: Per Chapter 134 of the Legislative Code, no person shall construct and maintain any projection or encroachment within the public right-of-way. Construction of the development that necessitates temporary use of the Right-of-Way (ROW) for construction purposes shall be limited to equipment, personnel, devices and apparatuses that are removable following construction. Encroachment permits will not be granted for devices such as tie backs, rock bolts, H-piles, lagging, timbers, sheet piling, etc. that the owner is seeking to abandon in the ROW. Section 3201.3 of the Minnesota Building Code defers final authority of encroachments into public rights-of-way/public property to the local authority. City Legislative Code governs management of the public rights-of-way. Provided such installations are approved by Public Works, footings may be allowed to encroach into City ROW no more than twelve (12) inches at depths below eight (8) feet as provided for in Minnesota Building Code Section 3202.1. Said encroachments would require an encroachment permit from the City per Chapter 134 of the Legislative Code. Encroachments into County or State ROW are not allowed unless authorization has been granted from said agency. Marshall Avenue is a County facility.

SIGNING: Signs regulating parking and/or traffic on private property shall be installed by the property owner or contractor outside of the public right-of-way (ROW). Removal of signs within the public ROW shall be completed by the City. New signs or the reinstallation of existing signs, as approved by Public Works Traffic Engineering, regulating parking and/or traffic in the public ROW for this development shall be installed by the City at the expense of the development. Contact Chris Gulden of Public Works 651-266-9778 two weeks in advance of needed sign work.

CONSTRUCTION IN RIGHT OF WAY: All work on curbs, driveways, and sidewalks within the public right of way must be done to City Standards and Specifications by a contractor licensed to work in the City right-of-way under a permit from Public Works Sidewalk Section (651-266- 6108). Sidewalk grades must be carried across driveways. Please coordinate any needed short term lane closures with Metro Transit Street Operations, Greg Tuveson (612-349-5407 / greg.tuveson@metrotransit.org)

RIGHT OF WAY RESTORATION: Restoration of asphalt and concrete pavements are performed by the Public Works Street Maintenance Division. The contractor is responsible for payment to the City for the cost of these restorations. The contractor shall contact Public Works Street Maintenance to set up a work order prior to beginning any removals in the street at 651- 266-9700. Procedures and unit costs are found in Street Maintenance's "General Requirements - All Restorations" and are available at the permit office.

SEWER REMOVAL/ABANDONMENT PERMIT: Plumbing Contractor to obtain "Removal Permits" from Public Works to cut off existing sewer connection services to the property. Services A11882 & A12719 will need a removal permit. Services will be cut off at the property line. Call St Paul PW permit desk (651-266-6234) for information on obtaining this permit.

TRAFFIC (cont.):
CITY OF ST. PAUL PERMIT REQUIREMENTS:
ORDERING OBSTRUCTION AND EXCAVATION PERMITS: Contact Public Works Right of Way Service Desk at (651) 266-6151. It is strongly recommended that contractors call for cost estimates prior to bidding to obtain accurate cost estimates.
OBSTRUCTION PERMITS: The contractor must obtain an Obstruction Permit if construction (including silt fences) will block City streets, sidewalks or alleys, or if driving over curbs.
EXCAVATION PERMITS: All digging in the public right of way requires an Excavation Permit. If the proposed building is close to the right of way, and excavating into the right of way is needed to facilitate construction, contact the utility inspector.
FAILURE TO SECURE PERMITS: Failure to secure Obstruction Permits or Excavation Permits will result in a double-permit fee and other fees required under City of St. Paul Legislative Codes.

WATER RESOURCE/EROSION CONTROL:
Adjacent streets and alleys must be swept to keep them free of sediment. Contractor must monitor conditions and sweep as needed or within 24 hours of notice by the City.

Project Location
Marshall Avenue & Moore Street
St. Paul, MN 55104

SYMBOL LEGEND

- REMOVE AND DISPOSE OF EXISTING BITUMINOUS PAVEMENT SECTION
- REMOVE AND DISPOSE OF EXISTING CONCRETE PAVEMENT SECTION
- REMOVE AND DISPOSE OF EXISTING PAVERS

DEMOLITION NOTES

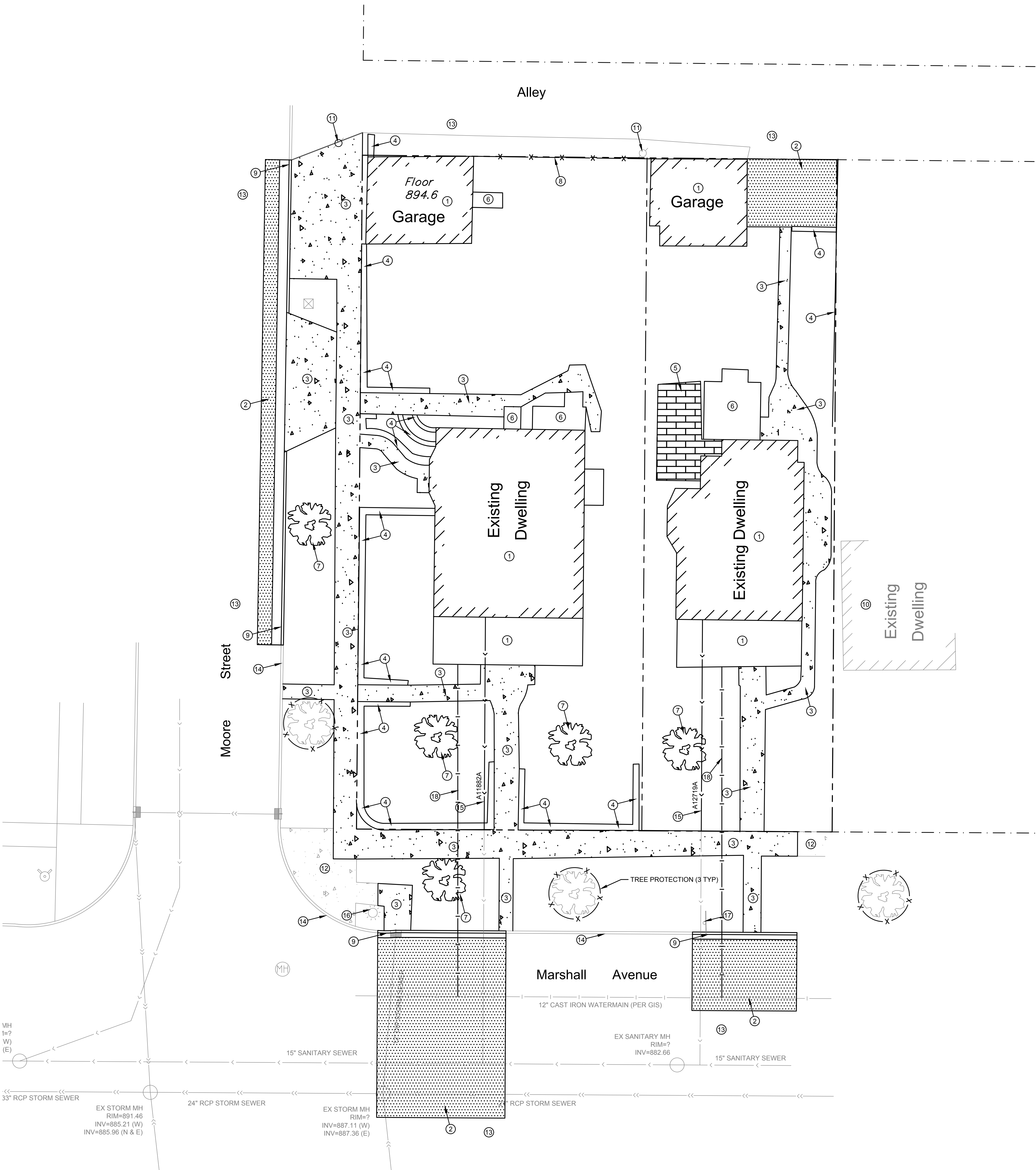
- Verify all existing utility locations.
- It is the responsibility of the Contractor to perform or coordinate all necessary utility demolitions and relocations from existing utility locations to all onsite amenities and buildings. These connections include, but are not limited to, water, sanitary sewer, cable tv, telephone, gas, electric, site lighting, etc.
- Prior to beginning work, contact Gopher State Onecall (651-454-0002) to locate utilities throughout the area under construction. The Contractor shall retain the services of a private utility locator to locate the private utilities.
- Sawcut along edges of pavements, sidewalks, and curbs to remain.
- All construction shall be performed in accordance with state and local standard specifications for construction.
- The removal, pruning, and/or planting of trees on the public boulevard requires an approved permit from the City Forester (651-632-2437). Any work must be completed by a licensed tree contractor.
- Construction supplies, materials, spoils, equipment, and vehicles shall not be stored or operated within the drip line of any public street tree or on turf boulevards without prior written approval from the City Forester. If the boulevard must be used for construction activities, site access routes, material storage or other related activities, protective measures approved by the City Forester shall be taken to reduce soil compaction and protect tree(s) from damage.
- The location of tree protection fences is shown on the plan. Tree protection fencing shall be installed prior to the start of any site work. Maintain the tree protection throughout the construction process. Any damage to the tree protection fence shall be repaired immediately. Trees damaged or removed during construction shall be resotted or replaced to the satisfaction of, and at no cost to, the City as determined by the Forestry manager.
- Contractor shall contact the City Forester (651-632-2437), prior to demolition or other land disturbance associated with site construction, to verify tree protection measures.
- Contractor shall protect existing boulevard infrastructure (signs, lighting, etc.) during construction. Any existing boulevard infrastructure items damaged during construction shall be replaced at the response of the contractor.

KEY NOTES

- REMOVE AND DISPOSE OF EXISTING BUILDING.
- REMOVE AND DISPOSE OF EXISTING BITUMINOUS PAVEMENT SECTION.
- REMOVE AND DISPOSE OF EXISTING CONCRETE PAVEMENT SECTION.
- REMOVE AND DISPOSE OF EXISTING RETAINING WALL.
- REMOVE AND DISPOSE OF EXISTING PAVERS.
- REMOVE AND DISPOSE OF EXISTING DECK.
- REMOVE AND DISPOSE OF EXISTING TREES, STUMPS AND ROOTS.
- REMOVE AND DISPOSE OF EXISTING FENCE.
- REMOVE AND DISPOSE OF EXISTING CONCRETE CURB AND GUTTER.
- PROTECT EXISTING BUILDING.
- PROTECT EXISTING POWER POLE.
- PROTECT EXISTING CONCRETE.
- PROTECT EXISTING BITUMINOUS.
- PROTECT EXISTING CURB AND GUTTER.
- REMOVE AND DISPOSE OF EXISTING SEWER SERVICE TO RIGHT OF WAY, THEN CAP EXISTING STUB.
- PROTECT OR REPLACE LIGHT POLE DURING CONSTRUCTION. (SEE TRAFFIC NOTES)
- SIGN TO BE REMOVED BY OTHERS (SEE TRAFFICS NOTES)
- REMOVE AND DISPOSE OF EXISTING WATER SERVICE.
- CUT OFF AT THE WATERMAIN. (FIELD VERIFY LOCATION)

SIDEWALKS:

All work on curbs, driveways, and sidewalks within the public right of way must be done to City Standards and Specifications by a licensed and bonded contractor under a permit from Public Works Sidewalk Section (651-266-6120). Sidewalk grades must be carried across driveways. Restoration of asphalt and concrete pavements are performed by the Public Works Street Maintenance Division. Contractor is responsible for damage to the mainline sidewalk, curb, drive access and boulevard landscaping cause during the construction. Contractor advised to document pre-existing condition of the right of way prior to commencement of the construction. The contractor is responsible for payment to the City for the cost of these restorations. The contractor shall contact Public Works Street Maintenance to set up a work order prior to beginning any removals in the street at 651-266-9700. Procedures and unit costs are found in Street Maintenance's "General Requirements - All Restorations" and are available at the permit office.



POPE
ARCHITECTS

POPE ARCHITECTS
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ST. PAUL, MN 55108-2735
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MCR Property
Holdings, LLC

**MARSHALL &
MOORE
APARTMENTS**
SAINT PAUL, MN
55104

Larson
Engineering, Inc.

3524 Labore Road
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651.481.9120 (t) 651.481.9201
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DEMOLITION
PLAN

Issues and Revisions:

I hereby verify that this plan, specifications or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the state of Minnesota.

Eric Meyer, P.E.
Eric Meyer, P.E.

Date: 12-28-2017 Reg. No.: 44592

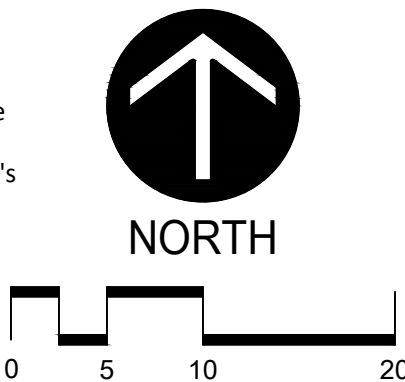
Commission No. 12176138

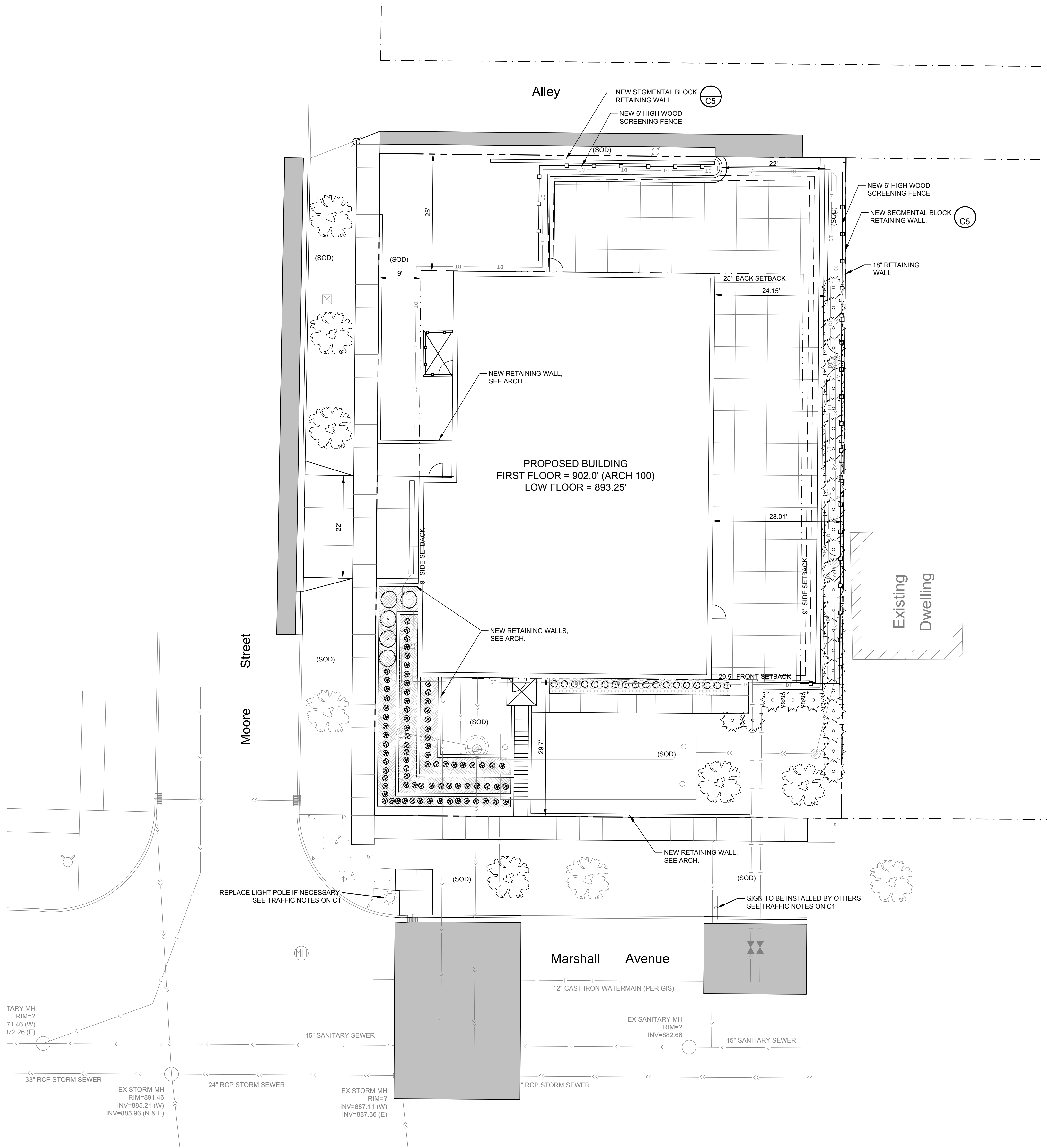
Drawn by KBK

Checked by EGM

SHEET

C1





SYMBOL LEGEND

- NEW BITUMINOUS PAVEMENT
- NEW CONCRETE PAVEMENT
- NEW STOOP / PATIO
SEE ARCHITECTURAL
- NEW GREEN ROOF
SEE ARCHITECTURAL
- NEW 4" WOOD MULCH OVER WEED BARRIER
- NEW 4" LANDSCAPE ROCK OVER WEED BARRIER
- NEW BOULEVARD TREE
(SWAMP WHITE OAK OR HACKBERRY)
- NEW PYRAMIDAL ARBORVITAE
- NEW NORTHWIND SWITCHGRASS
- NEW ALPINE CURRANT
- NEW OSTRICH FERN

IRRIGATION NOTES

- Irrigation system is design-build. Provide commercial grade drip-line irrigation in mulched and landscape rock planting beds and pop up heads in mowed turf areas. Provide shop drawings for review and approval.
- Install PVC Class 160 SDR-16 sleeves beneath hard surfaces. Extend sleeves 18 inches beyond hardscape. Sleeve sizes to be two sizes larger than irrigation piping and 4-inch for wiring.
- Install thrust blocking at all tees and bends.
- All fittings shall be Schedule 40.
- Provide 18 inches of wire slack at each valve.
- All wire splices, valves, and quick couplers shall be located in valve boxes.
- Irrigation contractor is responsible for first year's winterizing and the following spring start up.

LANDSCAPING NOTES

- Provide 24" of planting medium at all landscape beds (60% sand, 25% loam, 15% compost).
- Provide 6" of imported, processed topsoil for all sod areas.
- Trees planted on the boulevard are to be 2" caliper and have a minimum of a one year warranty.
- Where driveways, sidewalks or other surface paving are removed all concrete, asphalt and base materials shall be removed.
- Boulevard soils are to be protected during construction. Soil compaction due to construction activities shall be mitigated and soils loosened prior to final grading.



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www.larsonengr.com

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PAVING AND LANDSCAPING PLAN

Issues and Revisions:

I hereby certify that this plan, specifications or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the state of Minnesota.

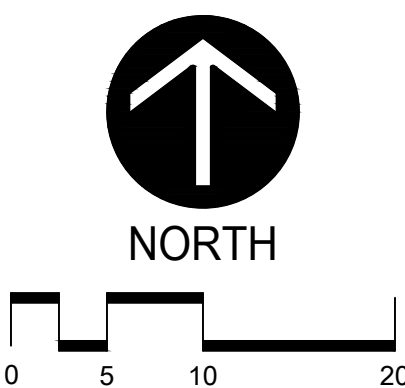
Eric Meyer, P.E.
Date: 12-28-2017 Reg. No.: 44592

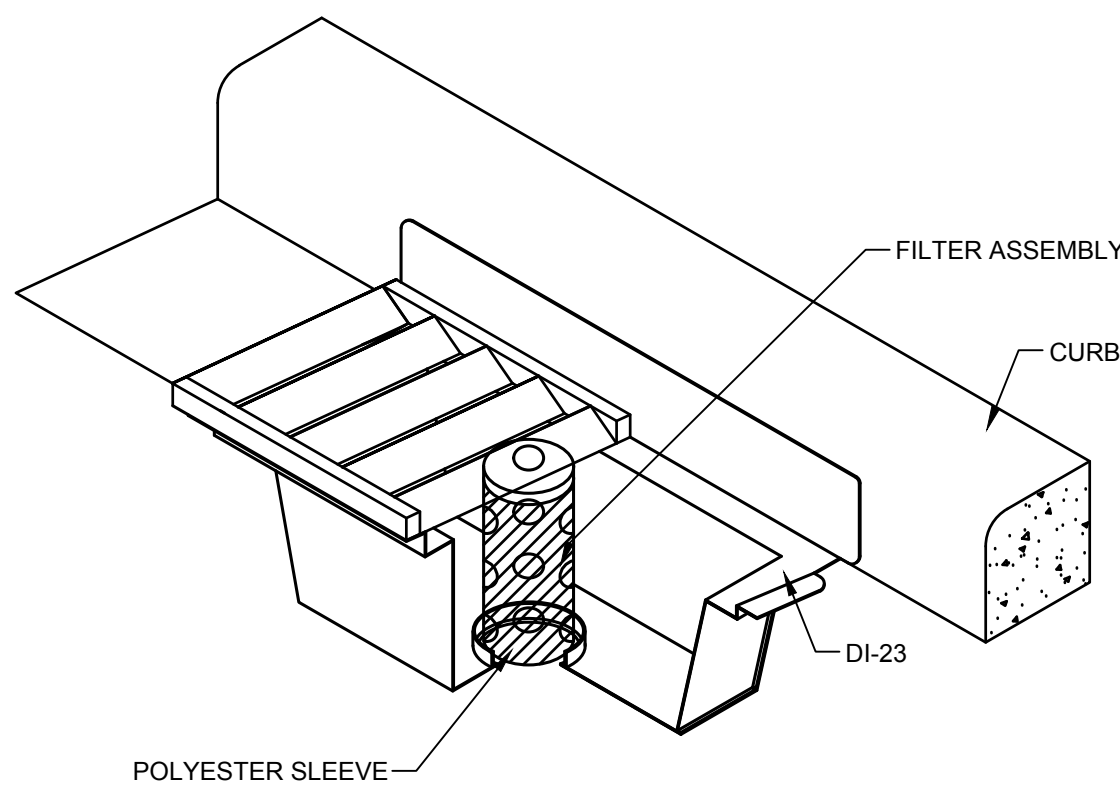
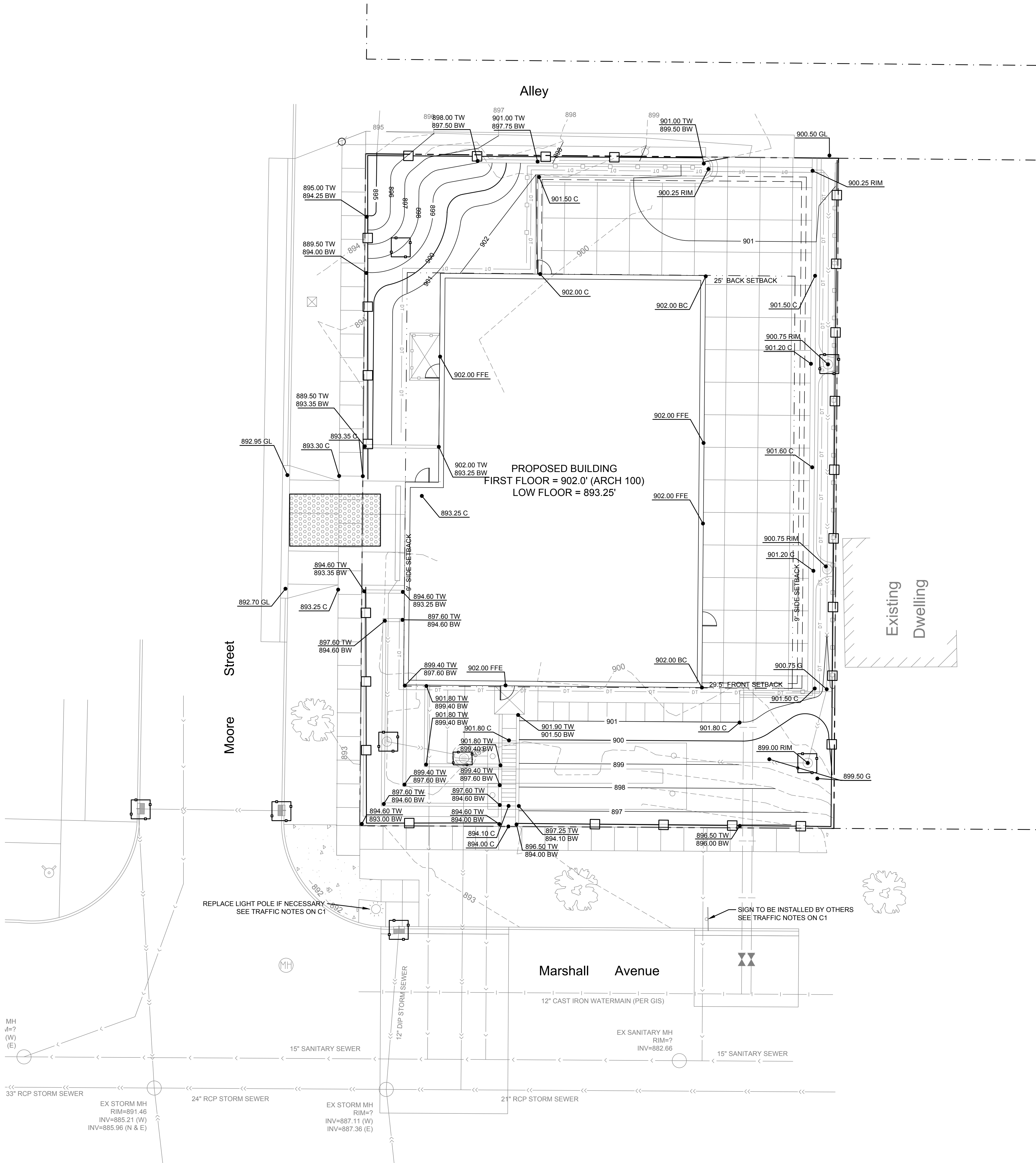
Commission No. 12176138
Drawn by KBK
Checked by EGM

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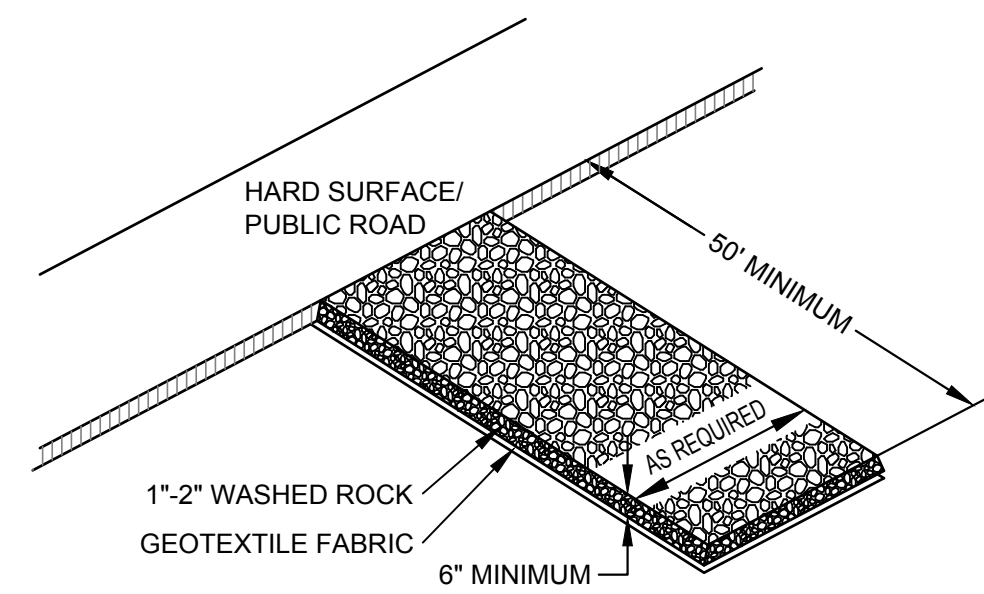
C2

THIS SHEET SCALE
0" 10" 20"

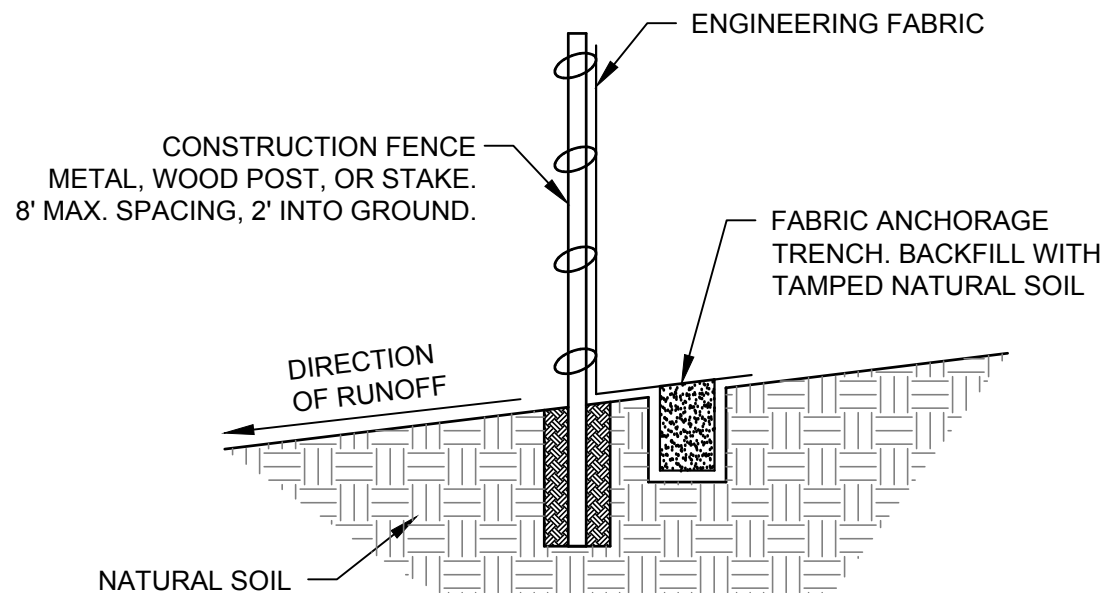




WIMCO INLET PROTECTION DEVICE OR EQUAL
NOT TO SCALE



ROCK CONSTRUCTION ENTRANCE
NOT TO SCALE



SILT FENCE INSTALLATION DETAIL
NOT TO SCALE

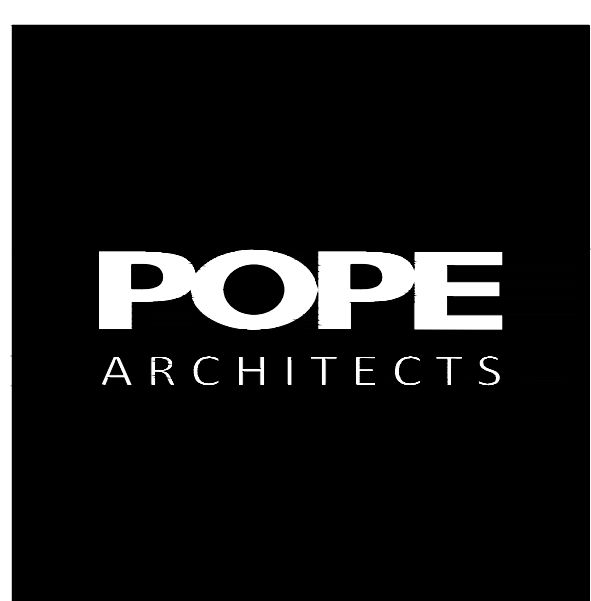
LEGEND	
	EXISTING CONTOURS
	PROPOSED CONTOURS - MAJOR INTERVAL
	PROPOSED CONTOURS - MINOR INTERVAL
	GRADE BREAK LINE
	GRADE SLOPE
	SILT FENCE
	RIP-RAP / ROCK CONST. ENTRANCE
	INLET PROTECTION
	CONCRETE WASHOUT STATION
SPOT ABBREVIATIONS:	
TC	TOP OF CURB
GL	GUTTER LINE
B	BITUMUNOUS
C	CONCRETE
EO	EMERGENCY OVERFLOW
G	GROUND
ME	MATCH EXISTING
(*)	EXISTING TO BE VERIFIED

EROSION CONTROL NOTES

1. Install temporary erosion control measures (inlet protection, silt fence, and rock construction entrances) prior to beginning any excavation or demolition work at the site.
2. Erosion control measures shown on the erosion control plan are the absolute minimum. The contractor shall install temporary earth dikes, sediment traps or basins, additional siltation fencing, and/or disk the soil parallel to the contours as deemed necessary to further control erosion.
3. All construction site entrances shall be surfaced with crushed rock across the entire width of the entrance and from the entrance to a point 50' into the construction zone.
4. The toe of the silt fence shall be trenched in a minimum of 6". The trench backfill shall be compacted with a vibratory plate compactor.
5. All grading operations shall be conducted in a manner to minimize the potential for site erosion. Sediment control practices must be established on all down gradient perimeters before any up gradient land disturbing activities begin.
6. All exposed soil areas must be stabilized as soon as possible to limit soil erosion but in no case later than 14 days after the construction activity in that portion of the site has temporarily or permanently ceased. Temporary stockpiles without significant silt, clay or organic components (e.g., clean aggregate stockpiles, demolition concrete stockpiles, sand stockpiles) and the constructed base components of roads, parking lots and similar surfaces are exempt from this requirement.
7. The normal wetted perimeter of any temporary or permanent drainage ditch or swale that drains water from any portion of the construction site, or diverts water around the site, must be stabilized within 200 lineal feet from the property edge, or from the point of discharge into any surface water. Stabilization of the last 200 lineal feet must be completed within 24 hours after connecting to a surface water. Stabilization of the remaining portions of any temporary or permanent ditches or swales must be complete within 14 days after connecting to a surface water and construction in that portion of the ditch has temporarily or permanently ceased.
8. All storm sewer catch basins not needed for site drainage during construction shall be covered to prevent runoff from entering the storm sewer system. Catch basins necessary for site drainage during construction shall be provided with inlet protection.
9. In areas where concentrated flows occur (such as swales and areas in front of storm catch basins and intakes) the erosion control facilities shall be backed by stabilization structure to protect those facilities from the concentrated flows.
10. Inspect the construction site once every seven days during active construction and within 24 hours after a rainfall event greater than 0.25 inches in 24 hours.
11. All BMPs must be repaired, replaced, or supplemented when they become nonfunctional or the sediment reaches 1/3 of the capacity. These repairs must be made within 24 hours of discovery, or as soon as field conditions allow access.
12. If sediment escapes the construction site, off-site accumulations of sediment must be removed in a manner and at a frequency sufficient to minimize off-site impacts.
13. All soils tracked onto pavement shall be removed daily.
14. Temporary soil stockpiles must have silt fence or other effective sediment controls, and cannot be placed in surface waters, including stormwater conveyances such as curb and gutter systems, or condyts and ditches unless there is a bypass in place for the stormwater.
15. Collected sediment, asphalt and concrete millings, floating debris, paper, plastic, fabric, construction and demolition debris and other wastes must be disposed of properly and must comply with MPCA disposal requirements.
16. Oil, gasoline, paint and any hazardous substances must be properly stored, including secondary containment, to prevent spills, leaks or other discharge. Restricted access to storage areas must be provided to prevent vandalism. Storage and disposal of hazardous waste must be in compliance with MPCA regulations.
17. External washing of trucks and other construction vehicles must be limited to a defined area of the site. Runoff must be contained and waste properly disposed of. No engine degreasing is allowed onsite.
18. All liquid and solid wastes generated by concrete washout operations must be contained in a leak-proof containment facility or impermeable liner. A compacted clay liner that does not allow washout liquids to enter ground water is considered an impermeable liner. The liquid and solid wastes must not contact the ground, and there must not be runoff from the concrete washout operations or areas. Liquid and solid wastes must be disposed of properly and in compliance with MPCA regulations. A sign must be installed adjacent to each washout facility to inform concrete equipment operators to utilize the proper facilities.
19. Upon completion of the project and stabilization of all graded areas, all temporary erosion control facilities (silt fences, hay bales, etc.) shall be removed from the site.
20. All stormwater treatment facilities shall be restored to their design condition immediately following stabilization of the site.
21. Concrete washouts shall not be located within the drip line of a tree.

GRADING NOTES

1. Tree protection consisting of snow fence or safety fence installed at the drip line shall be in place prior to beginning any grading or demolition work at the site.
2. All elevations with an asterisk (*) shall be field verified. If elevations vary significantly, notify the Engineer for further instructions.
3. Grades shown in paved areas represent finish elevation.
4. All construction shall be performed in accordance with state and local standard specifications for construction.
5. Elevations are on NGVD 88 datum. NGVD 88 elevation 694.26 = St Paul 0.00.



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GRADING AND EROSION CONTROL PLAN

Issues and Revisions:

I hereby certify that this plan, specifications or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the state of Minnesota.

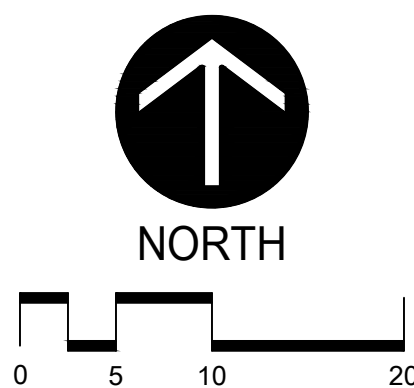
Eric Meyer, P.E.
Date: 12-28-2017 Reg. No.: 44592

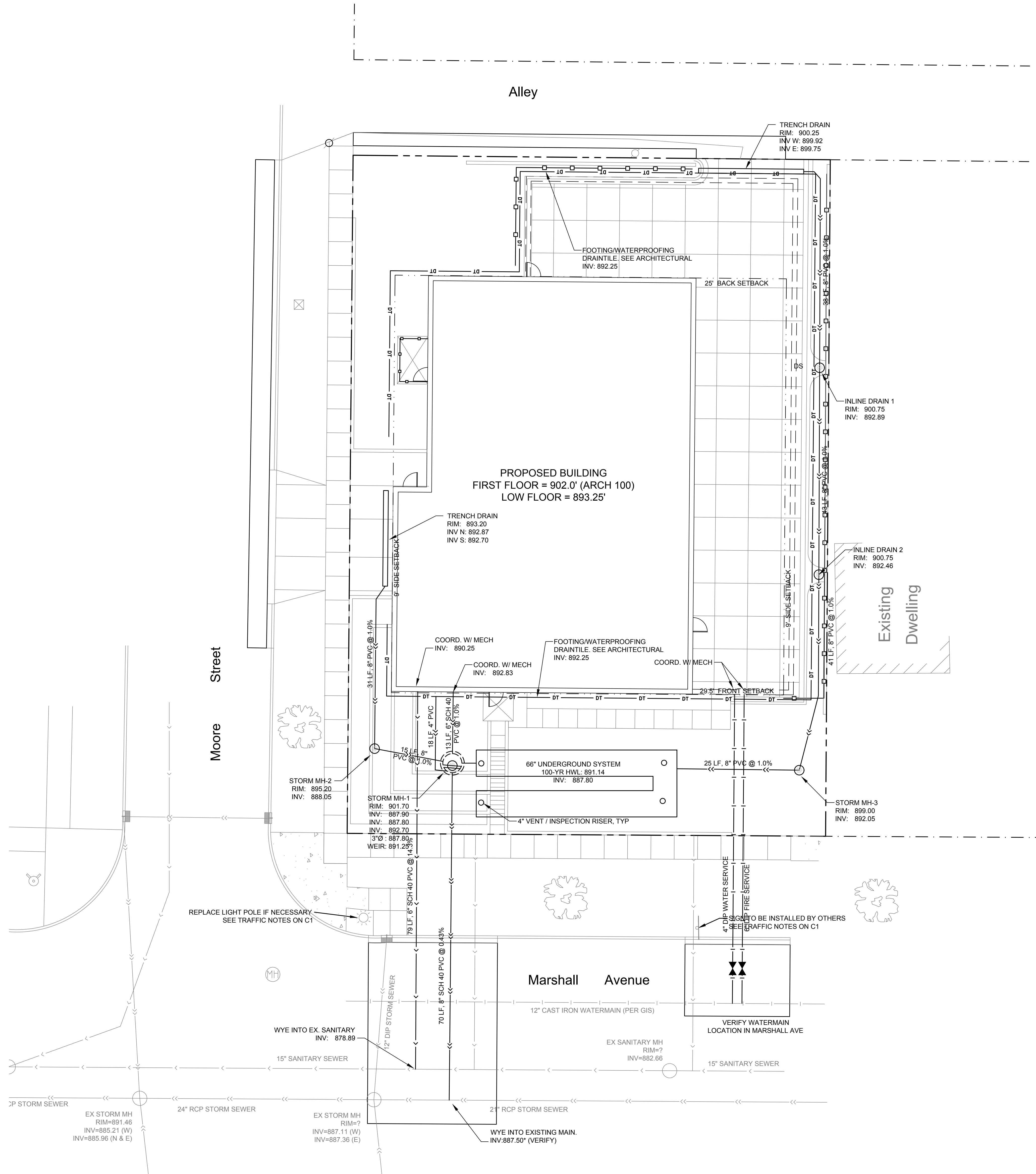
Commission No. 12176138
Drawn by KBK
Checked by EGM

SHEET

C3.0

THIS SHEET SCALE





PERMITTING

SEWER REPAIR PERMIT: Plumbing Contractor to obtain "Repair Permits" from Public Works for proposed modification to the existing storm sewer connections. Call St Paul PW permit desk (651-266-6234) for information on obtaining this permit.

SEWER REMOVAL/ABANDONMENT PERMIT: Plumbing Contractor to obtain "Removal Permits" from Public Works to cut off existing sewer connection services to the property. Services A11882 & A12719 will need a removal permit. Call St Paul PW permit desk (651-266-6234) for information on obtaining this permit.

SEWER CONNECTION PERMIT: License house drain contractor to obtain (Sewer Connection Permit) to construct new sanitary and storm connection in street from main to the property. Call St Paul PW permit desk (651-266-6234) for information on obtaining this permit.

Contact Saint Paul Regional Water Services (SPRWS) for questions, permits, fees, inspections, specifications, plans, or information that may be required for the water service and/or the water meter.

Sanitary and/or storm sewer services passing within 10 feet of the building are governed by the MN Plumbing Code. Specification for pipe material selection and notes for required air test of the piping, compliant with MN State Plumbing Code 4714 Section 11090, must be shown on the plan. This system must be reviewed and approved by Rick Jacobs, Senior Plumbing Inspector (651-266-9051) at the time of the Plumbing Plan review to ensure that it meets Plumbing Code standards.

Contact the City of Saint Paul Department of Public Works Sewer Division for questions, permits, fees, inspections, specifications, plans, or information that may be required for sewer and storm piping work performed outside the building, including "retention systems".

All primary roof drains shall be piped internally to below grade, exit the building, and be connected to the storm sewer. MPC 4714.1101.1.

Secondary Roof Drainage shall drain to an approved place of disposal in the form of Secondary Roof Drains installed per MPC 4714.1101 & 1102, and Minnesota State Building Code 1503.4.1-5. Secondary roof must discharge onto permeable soils and cannot drain onto the sidewalk. MPC 4714.1101.1. An approved place of disposal that can be approved by the Authority Having Jurisdiction for secondary roof drainage is in form of secondary roof drains piped internally, down to within 18 inches of grade, through the outside wall, onto a splash block installed per MPC 1101.5.3, and laid over permeable soils of an adequate amount where saturation of the soil will not occur.

LEGEND

○	STORM MANHOLE	— CTV —	CABLE UNDERGROUND LINE
○	CATCH BASIN	— E —	ELECTRIC OVERHEAD LINE
□	CURB INLET	— F0 —	ELECTRIC UNDERGROUND LINE
▲	FLARED END	— F —	FIBER OPTIC UNDERGROUND LINE
○	SANITARY MANHOLE	— N —	NATURAL GAS UNDERGROUND LINE
○	HYDRANT	— S —	SANITARY SEWER PIPE
⊗	GATE VALVE & BOX	— S —	STORM SEWER PIPE
⊗	WATER SHUTOFF	— T —	TELEPHONE UNDERGROUND LINE
☀	LIGHT POLE	— W —	WATERMAIN PIPE
		— D —	DRAINTILE PIPE

UTILITY NOTES

- All sanitary manholes to be City of St. Paul Type 1. (St. Paul std. Plate 2101B).
- All sanitary service Y-connections shall be ductile iron pipe.
- Sanitary service runs shall be PVC, schedule 40.
- Abandoning existing sewer service or making new connections to City sewer must be done to City standards by a Licensed House Drain Contractor under a permit from Public Works Sewer Section. Call 651-266-6234 for inspections.
- All unused existing water services to be cut off by St. Paul Regional Water Service.
- 6" water service to be installed by St. Paul Regional Water Service.
- All unused existing water services to be cut off by SPRWS. Excavation and restoration by owner's contractor. New water services will not be turned on until required cutoffs have been performed.
- Refer to SPRWS "Standards for the Installation of Water Mains" Standard Plate D-11 for restrained pipe requirement.
- All water service valve boxes within construction area must be exposed and brought to grade upon completion of construction.
- Pipe material for 8" Ductile Iron Pipe must be Class 52. Pipe material for 6" and 4" Ductile Iron Pipe must be Class 53. The exterior of ductile iron pipe shall be coated with a layer of arc-sprayed zinc per ISO 8179. The interior cement mortar lining shall be applied without asphalt seal coat.
- Maintain 8 feet of cover over all water mains and services.
- Maintain 3 feet vertical separation between water and sewer pipes or a 12 inch separation with 4 inch high density insulation per SPRWS Standard Plate D-10 for typical water main offsets.
- The contractor must obtain an obstruction permit from public works, Tony Dagostino, if trucks/equipment will be driving over curbs or if construction will block city streets, sidewalks, or alleys.
- Adjacent streets and alleys must be swept to keep them free of sediment. Contractor must monitor conditions and sweep as needed or within 24 hours of notice by the City.
- All storm sewer RCP pipe shall be class 5.
- See Project Specifications for bedding requirements.
- All new storm sewer manholes shall be by the City of St. Paul std. Plate #2104C-Type 4.
- New storm sewer catch basins shall be City of St. Paul std. Plate #2015E-Type 7A.
- All pipe work inside of property to be performed by a plumber licensed by the State of Minnesota and Certified by the City of Saint Paul. SPRWS requires separate outside and inside plumbing permits for each new water service.
- Water services to be installed according to SPRWS "Standards for the Installation of Water Mains".
- Water facility pipework within right of way to be installed by SPRWS. Excavation and restoration by owner's contractor.
- Elevations are on NGVD 88 datum. NGVD 88 elevation 694.26 = St Paul 0.00.
- A four-sided trench box is required on all excavations deeper than 5 feet where underground work or inspection is to be performed by SPRWS. Ladders are required and must extend 3 feet above the surface of the trench. Sidewalks, pavements, ducts and appurtenant structures shall not be undermined unless a support system or another method of protection is provided. Trenches in excess of 20 feet in depth must be signed off by a registered professional engineer. Excavated material must be kept a minimum of 2 feet from the edge of the trench.
- Pipe must be wrapped in V-Bio Polywrap encasement.
- The contractor providing excavation is responsible for obtaining all excavation and obstruction permits required by any governing authority.
- Contractor to maintain access to the fire department connection for fire department personnel at all times during the construction period.

POPE
ARCHITECTS

POPE ARCHITECTS
1295 BANDANA BLVD N, SUITE 200
ST. PAUL, MN 55108-2735
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www.popearch.com

MCR Property
Holdings, LLC

MARSHALL &
MOORE
APARTMENTS
SAINT PAUL, MN
55104

Larson
Engineering, Inc.

3524 Labore Road
White Bear Lake, MN 55110
651.481.9120 (T) 651.481.9201
www.larsonengr.com

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UTILITY PLAN

Issues and Revisions:

I hereby certify that this plan, specifications or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the state of Minnesota.

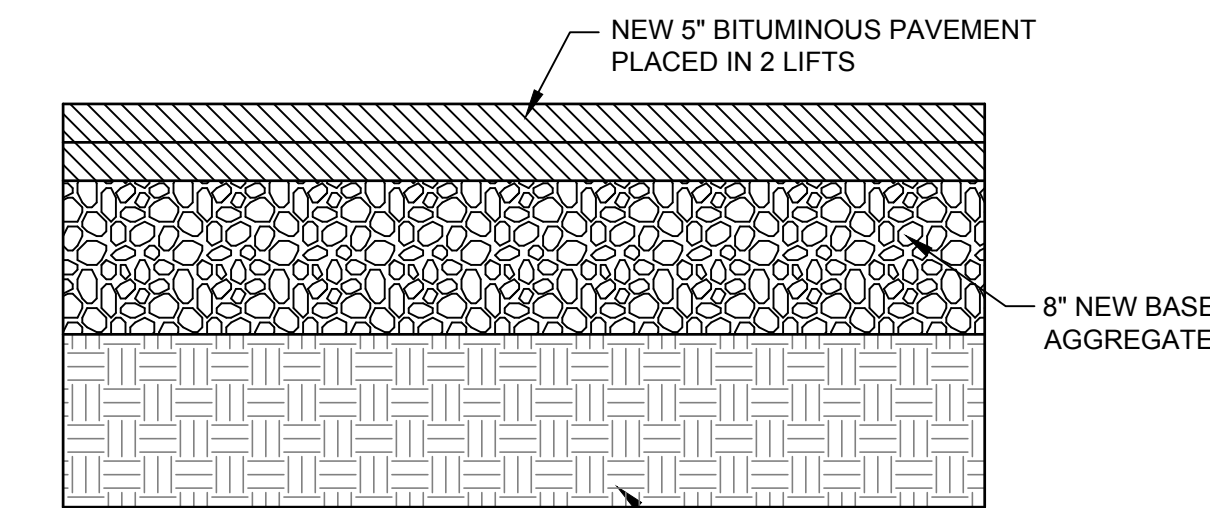
Eric Meyer, P.E.
Date: 12-28-2017 Reg. No.: 44592

Commission No. 12176138
Drawn by KBK
Checked by EGM

SHEET

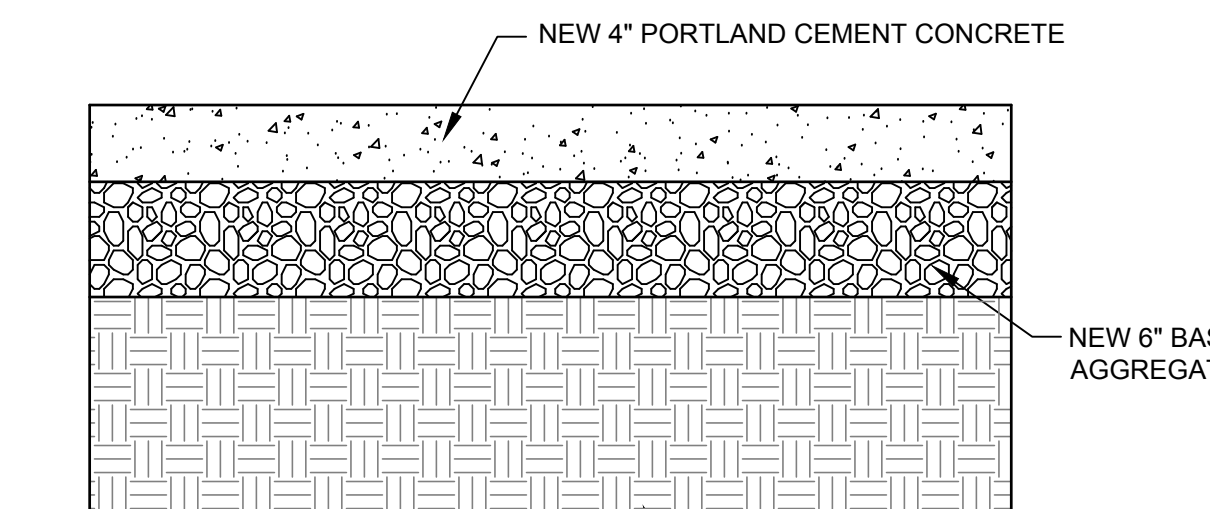
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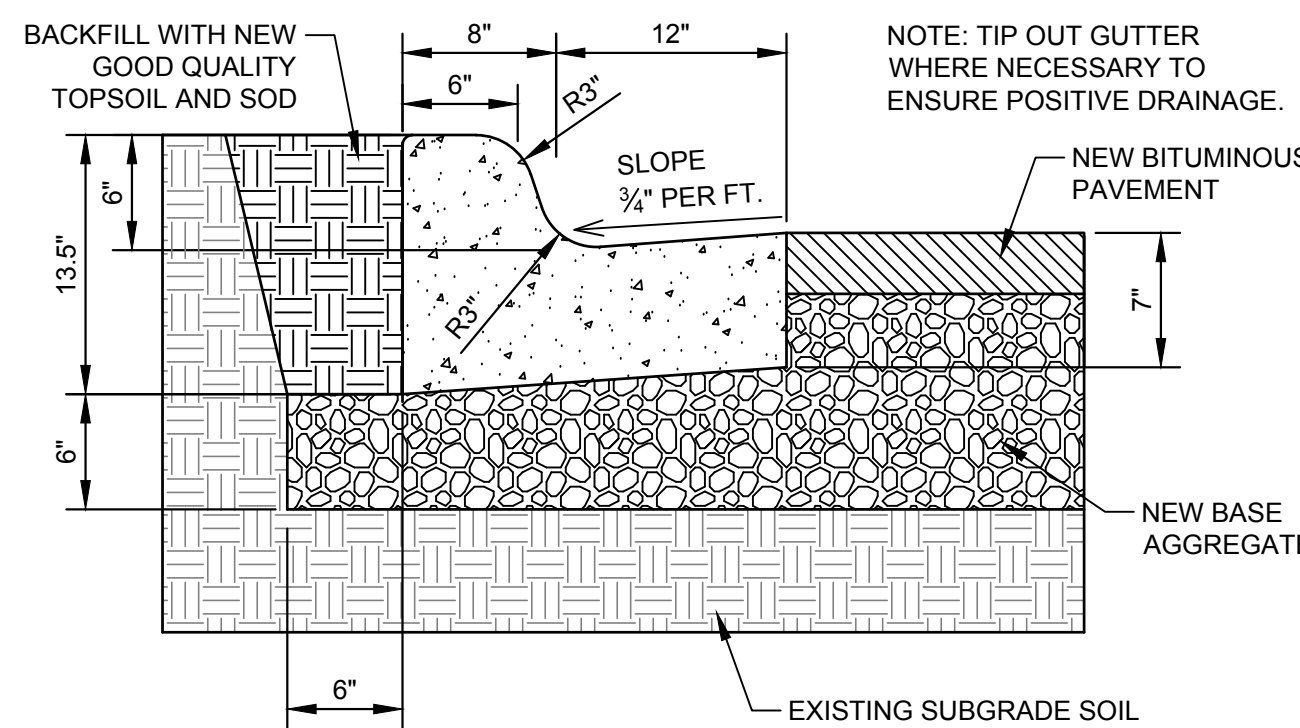
BITUMINOUS CONSTRUCTION DETAIL

NOT TO SCALE



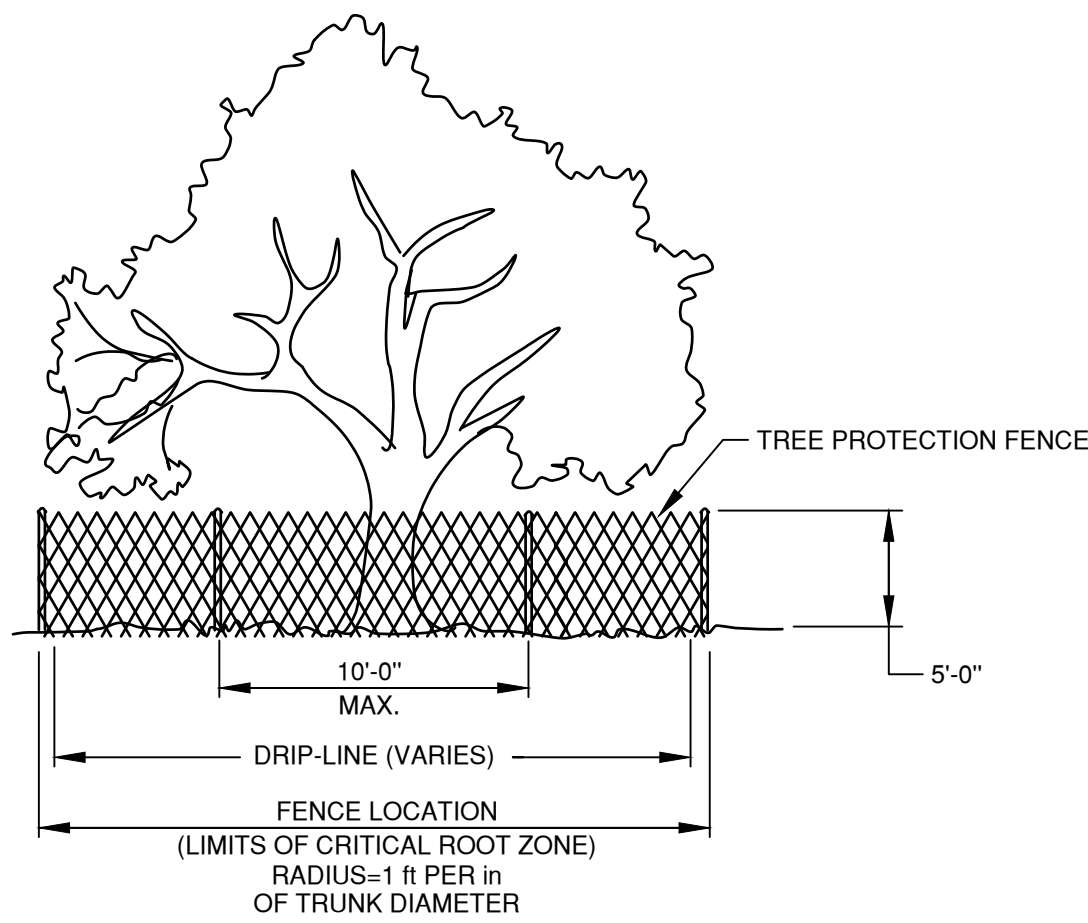
CONCRETE CONSTRUCTION DETAIL

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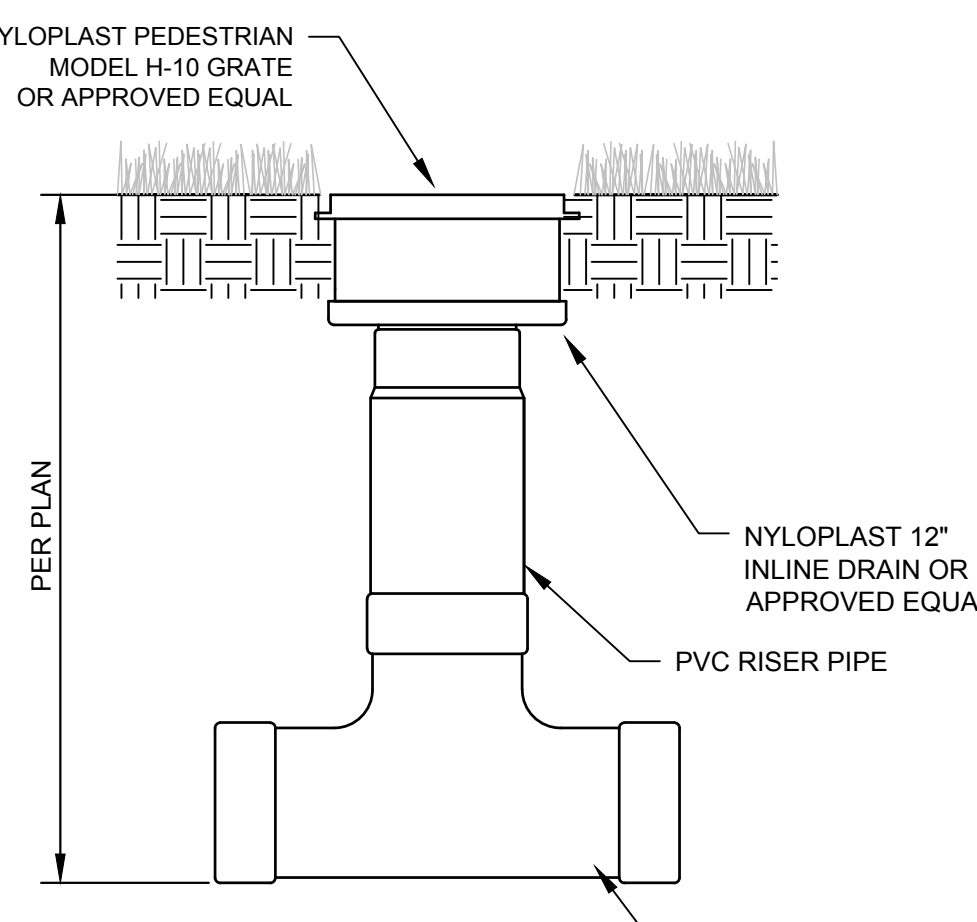
B612 CONCRETE CURB & GUTTER DETAIL

NOT TO SCALE



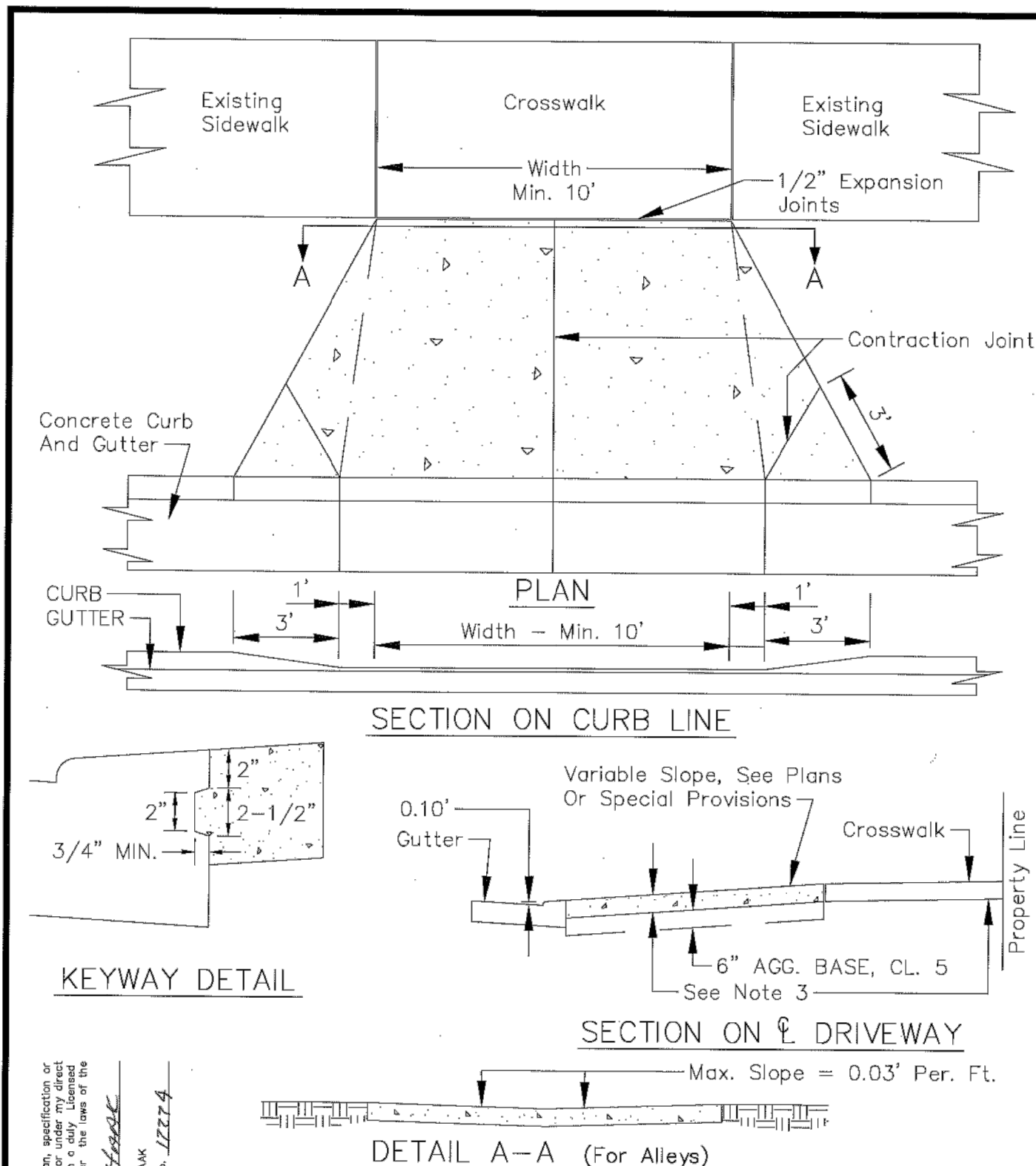
TREE PROTECTION

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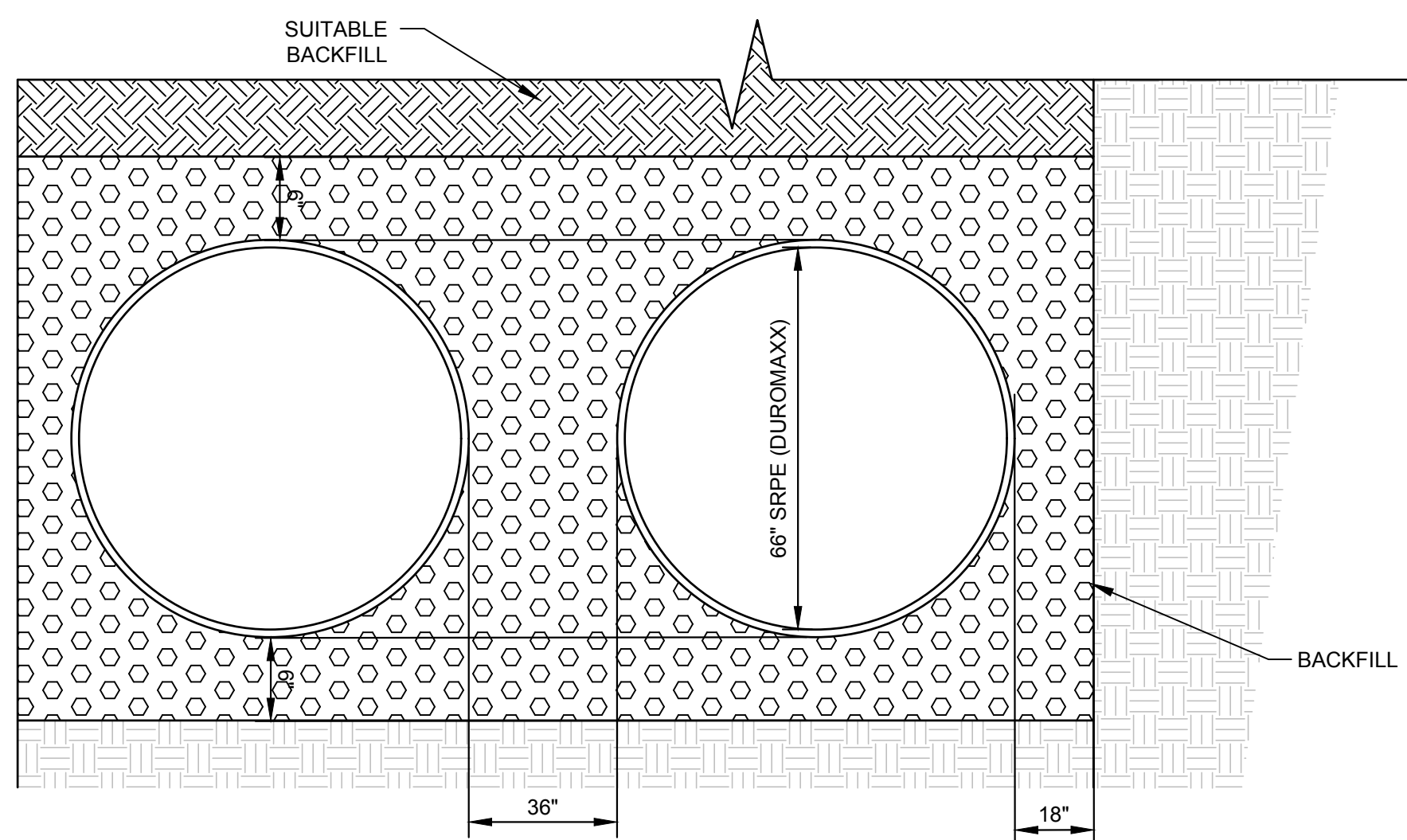
INLINE DRAIN DETAIL

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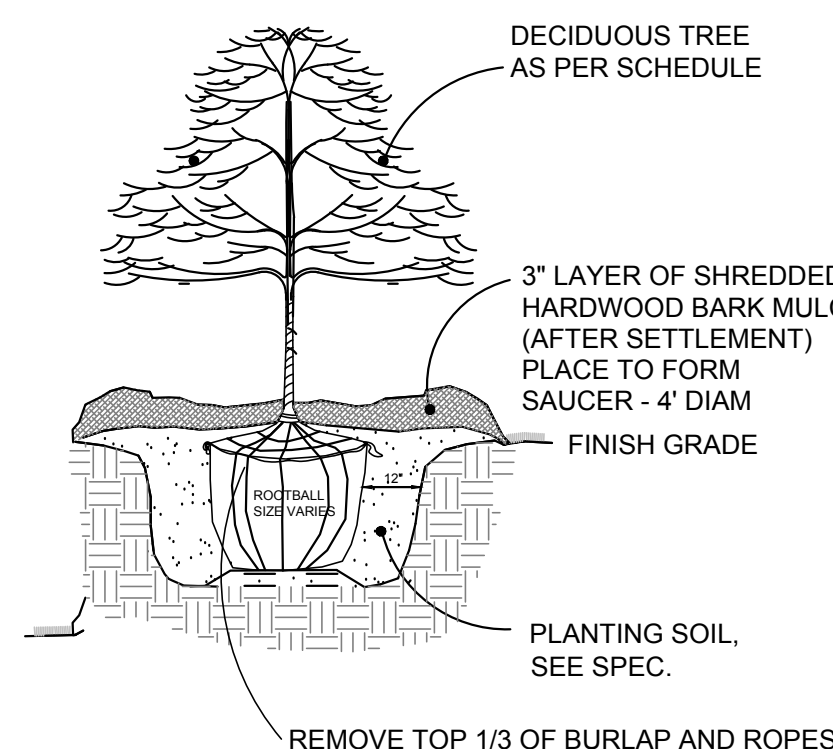
CONCRETE DRIVEWAY TYPE 6

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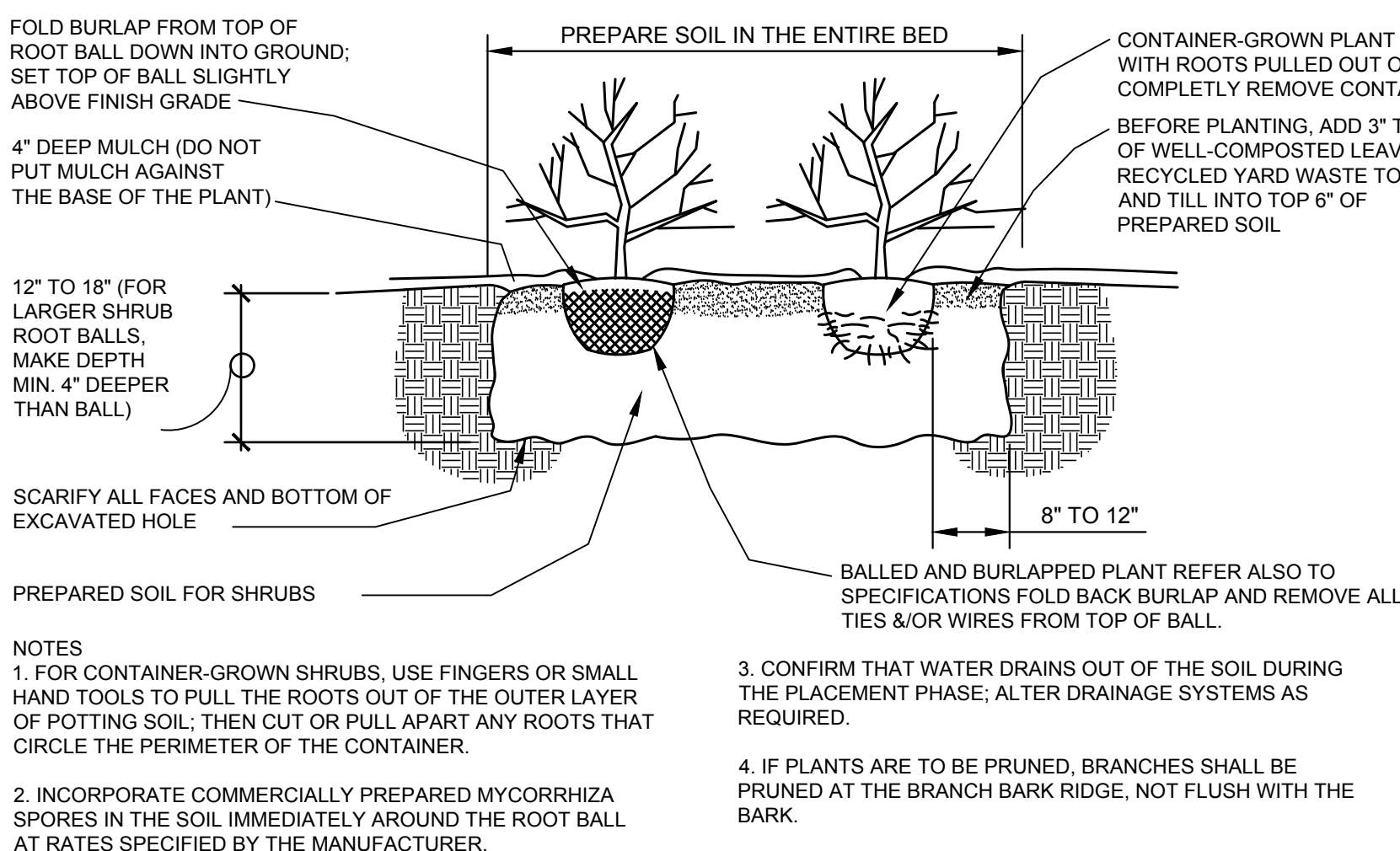
UNDERGROUND STORAGE SECTION

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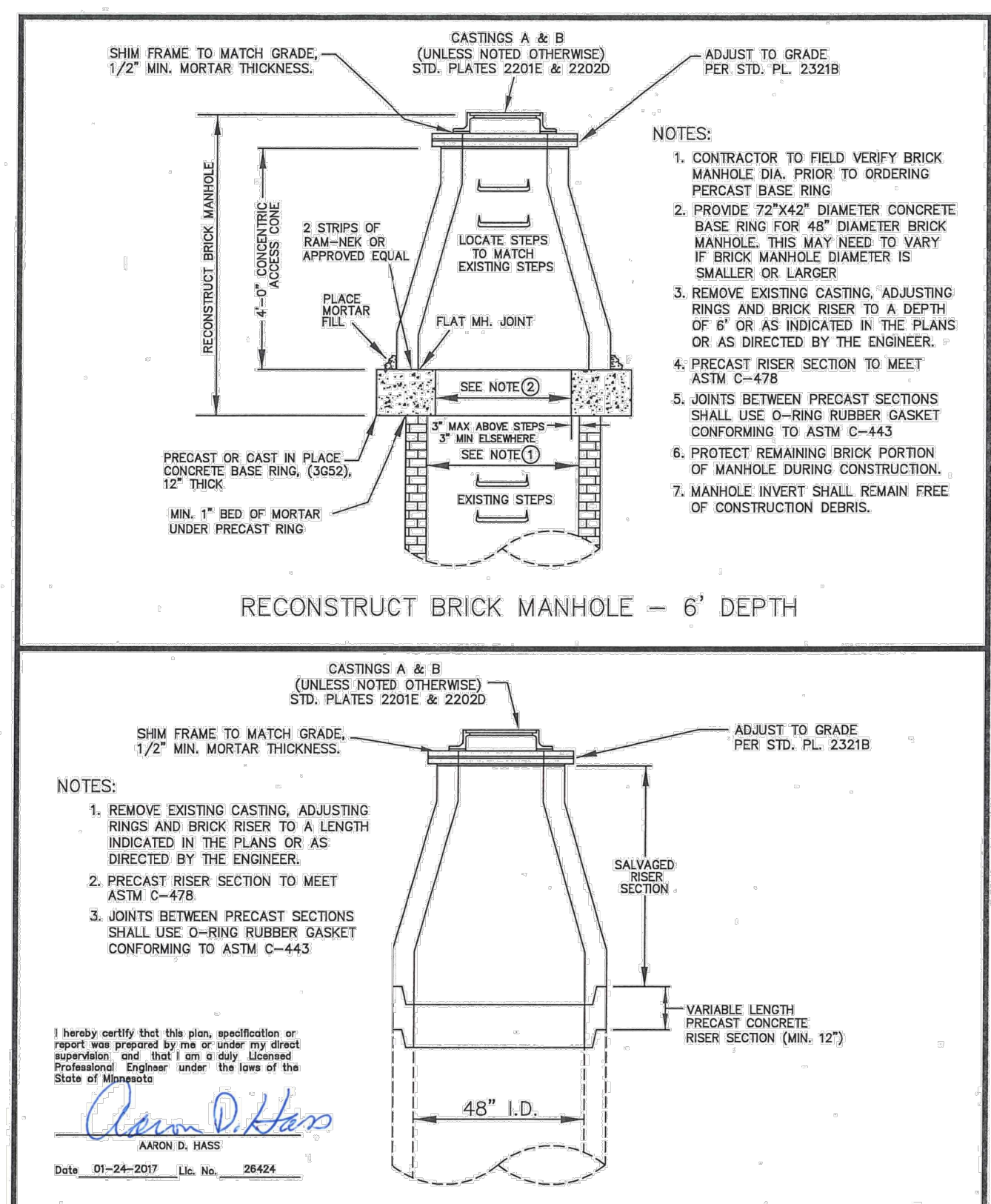
DECIDUOUS TREE PLANTING DETAIL

NOT TO SCALE



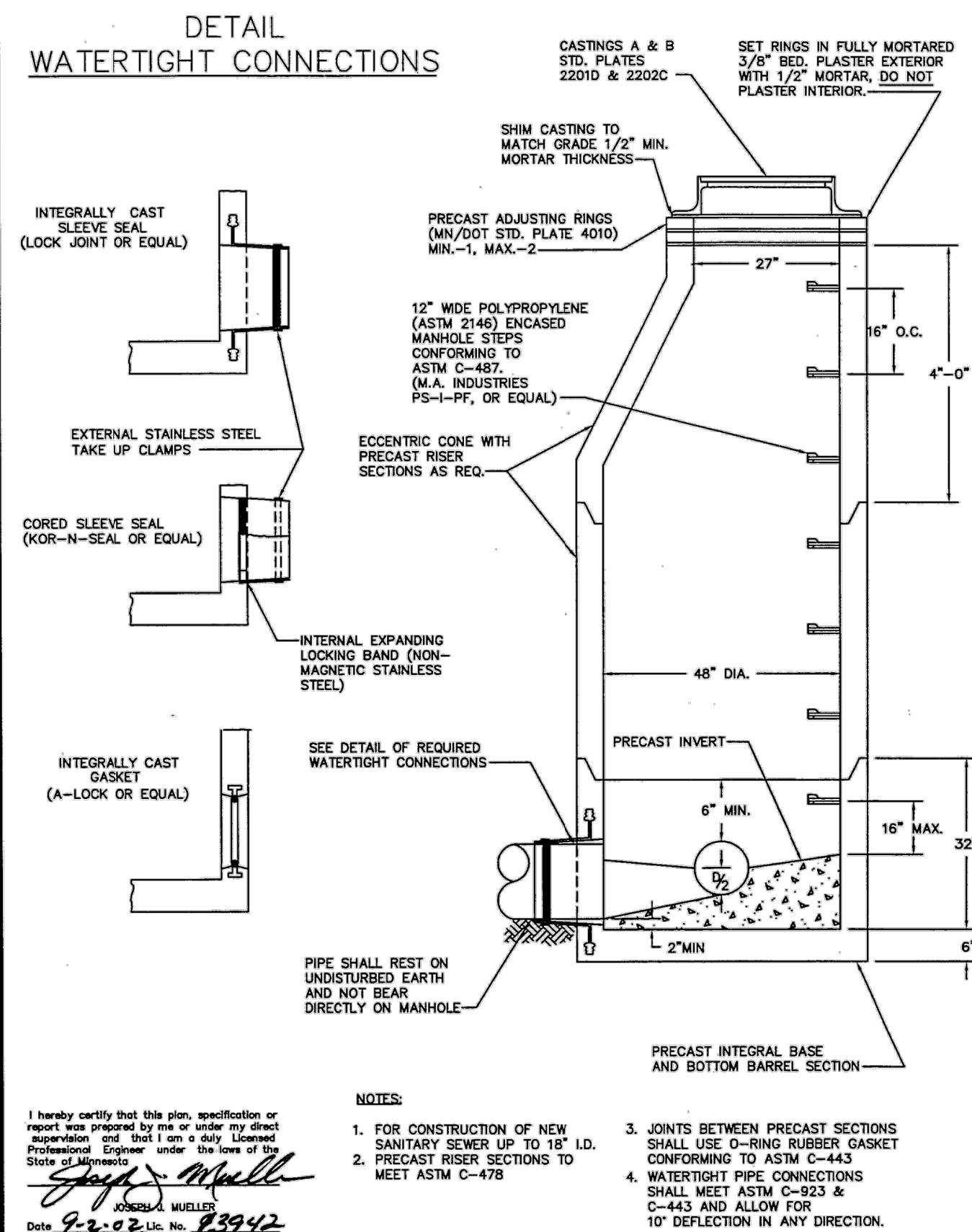
DECIDUOUS OR EVERGREEN SHRUB PLANTING DETAIL

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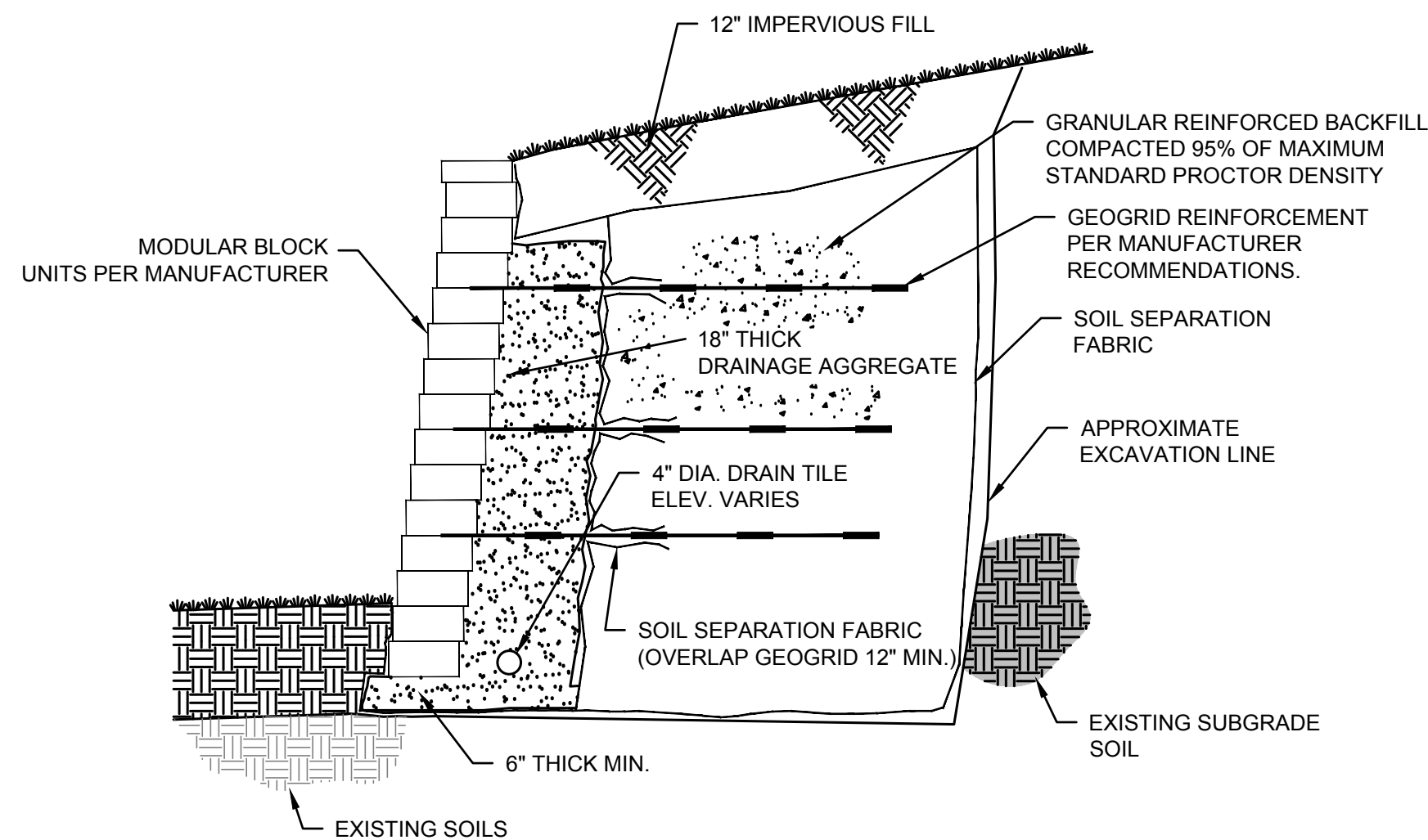
RECONSTRUCT BRICK MANHOLE - 6' DEPTH

NOT TO SCALE



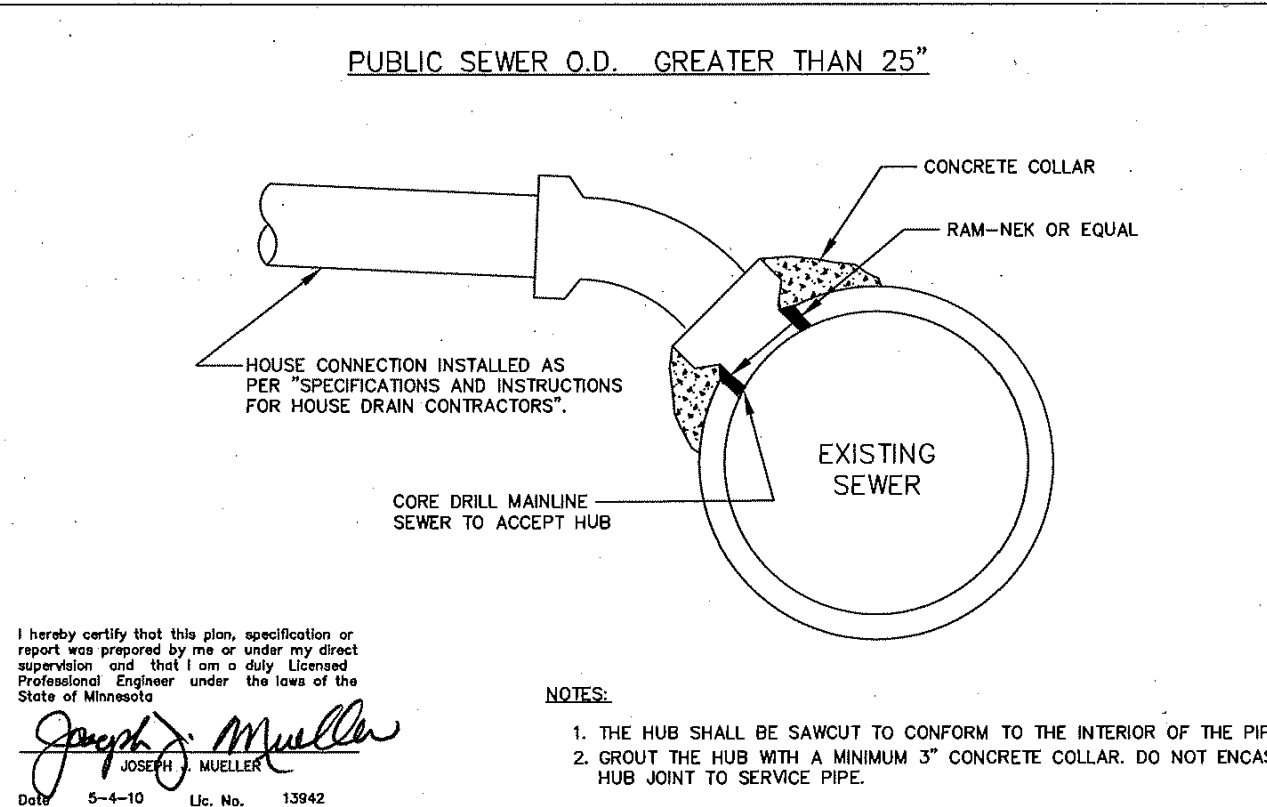
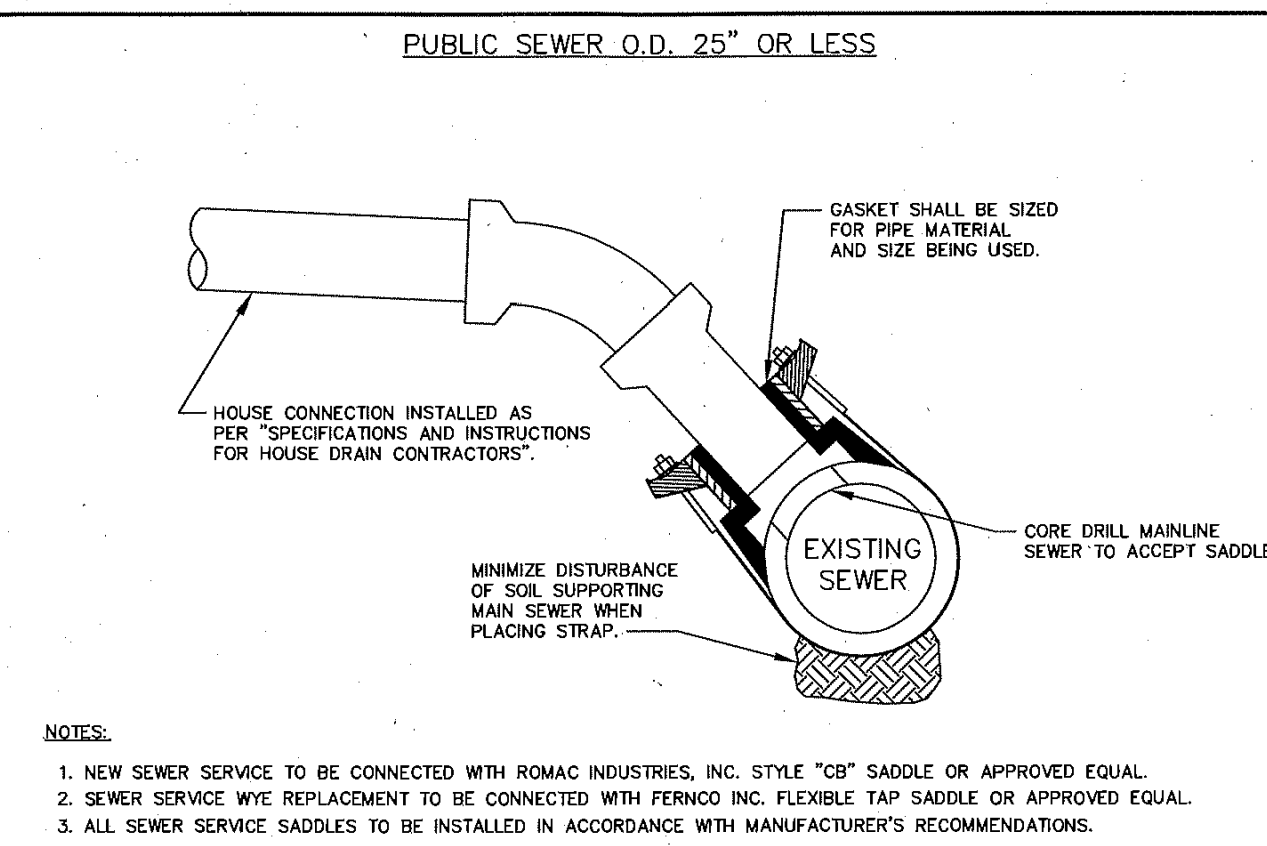
WATERTIGHT CONNECTIONS

NOT TO SCALE



RETAINING WALL DETAIL

NOT TO SCALE



PUBLIC SEWER O.D. GREATER THAN 25"

NOT TO SCALE

POPE
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DETAILS

Issues and Revisions:

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Eric Meyer, P.E.
Date: 12-28-2017 Reg. No.: 44592

Commission No. 12176138
Drawn by KBK
Checked by EGM

SHEET

C5

THIS SHEET SCALE



NEW
CONSTRUCTION
MARSHALL AVE
APARTMENTS
SAINT PAUL, MN
55104



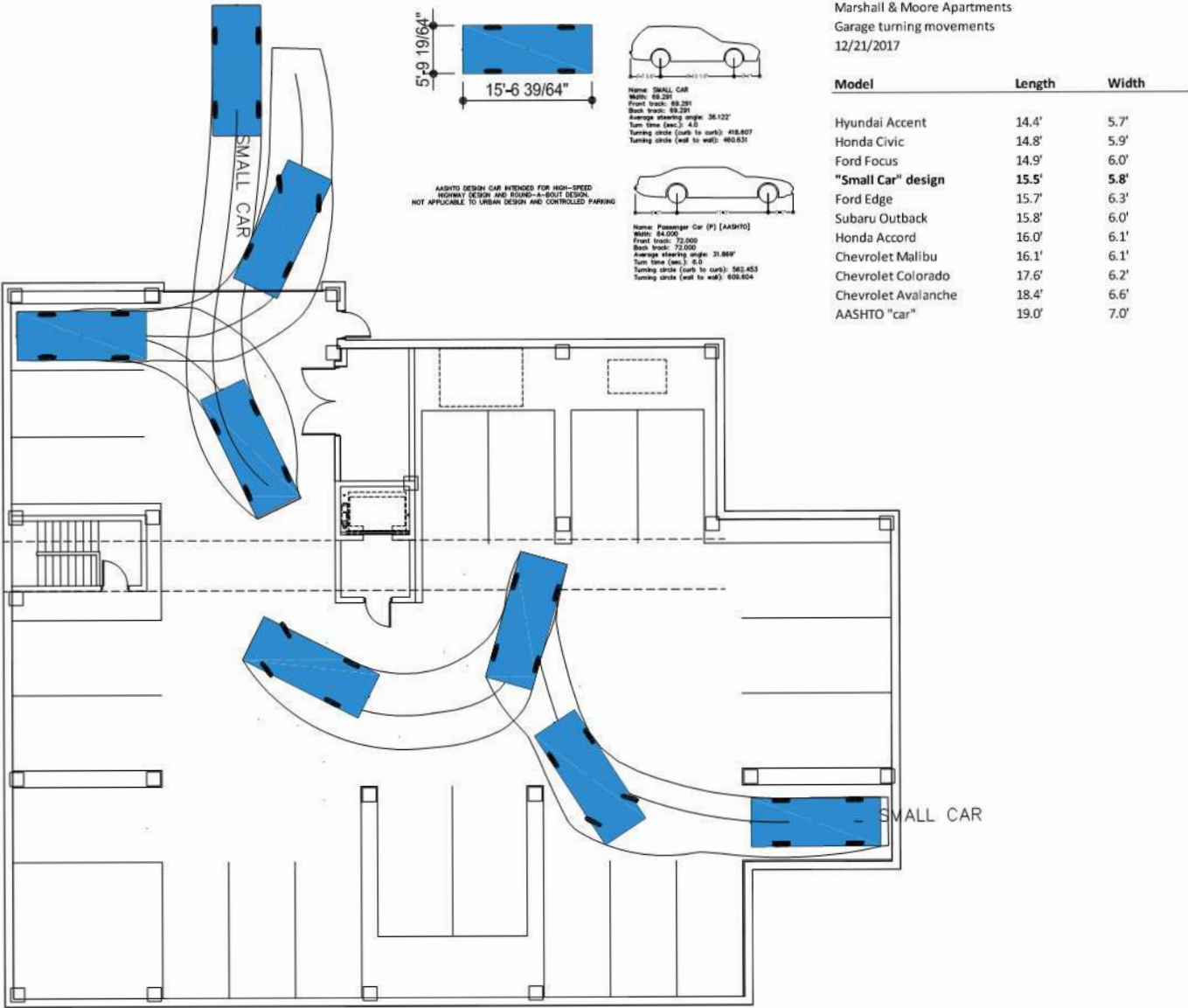
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4.100



MCR Property Holdings, LLC
MARSHALL & MOORE APARTMENTS
SAINT PAUL, MINNESOTA

SITE PLAN REVIEW
12/28/2017
51666-17088

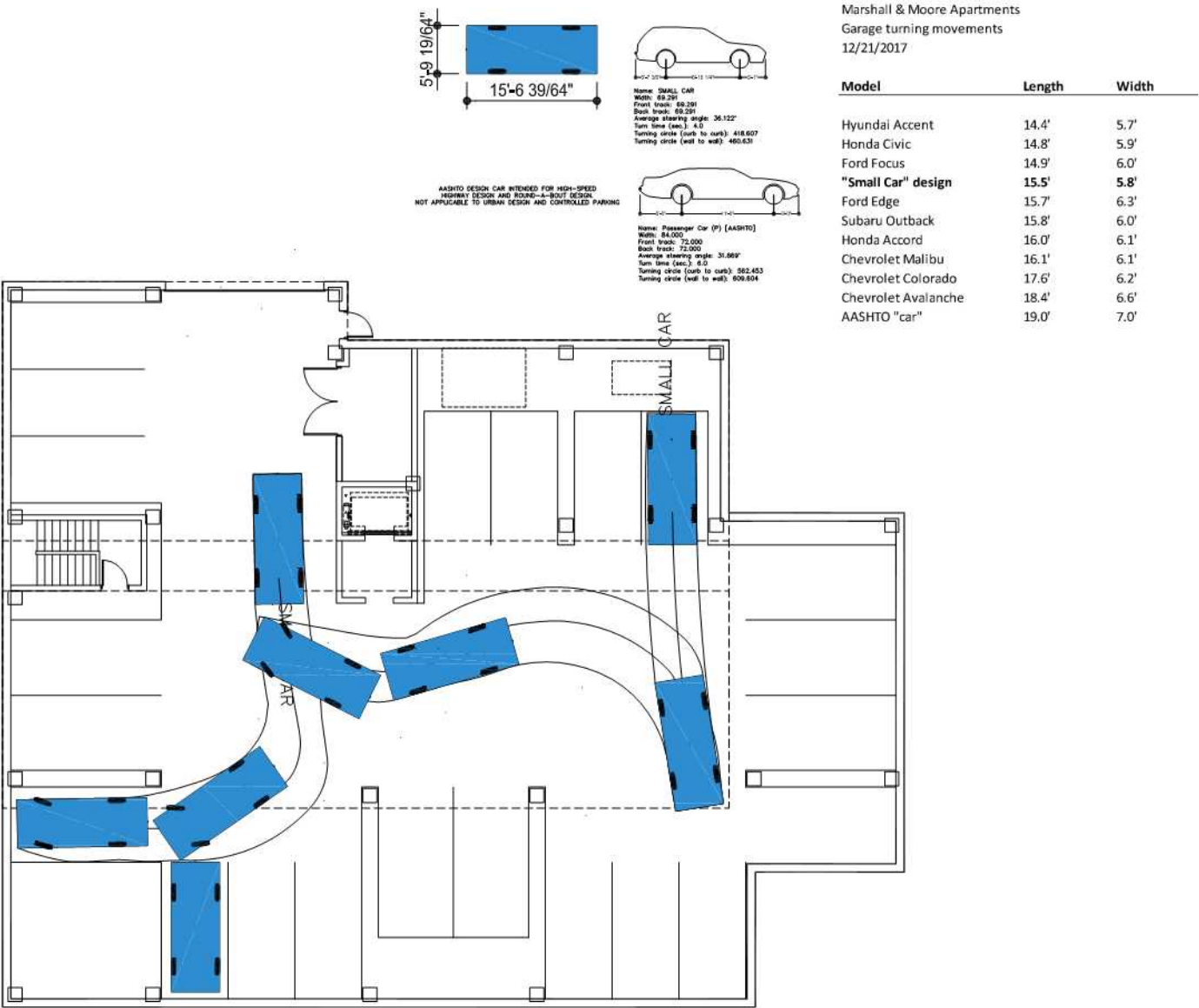


LOWER LEVEL 1

VEHICLE TURNING DIAGRAMS

MCR Property Holdings, LLC
MARSHALL & MOORE APARTMENTS
SAINT PAUL, MINNESOTA

SITE PLAN REVIEW
12/28/2017
51666-17088

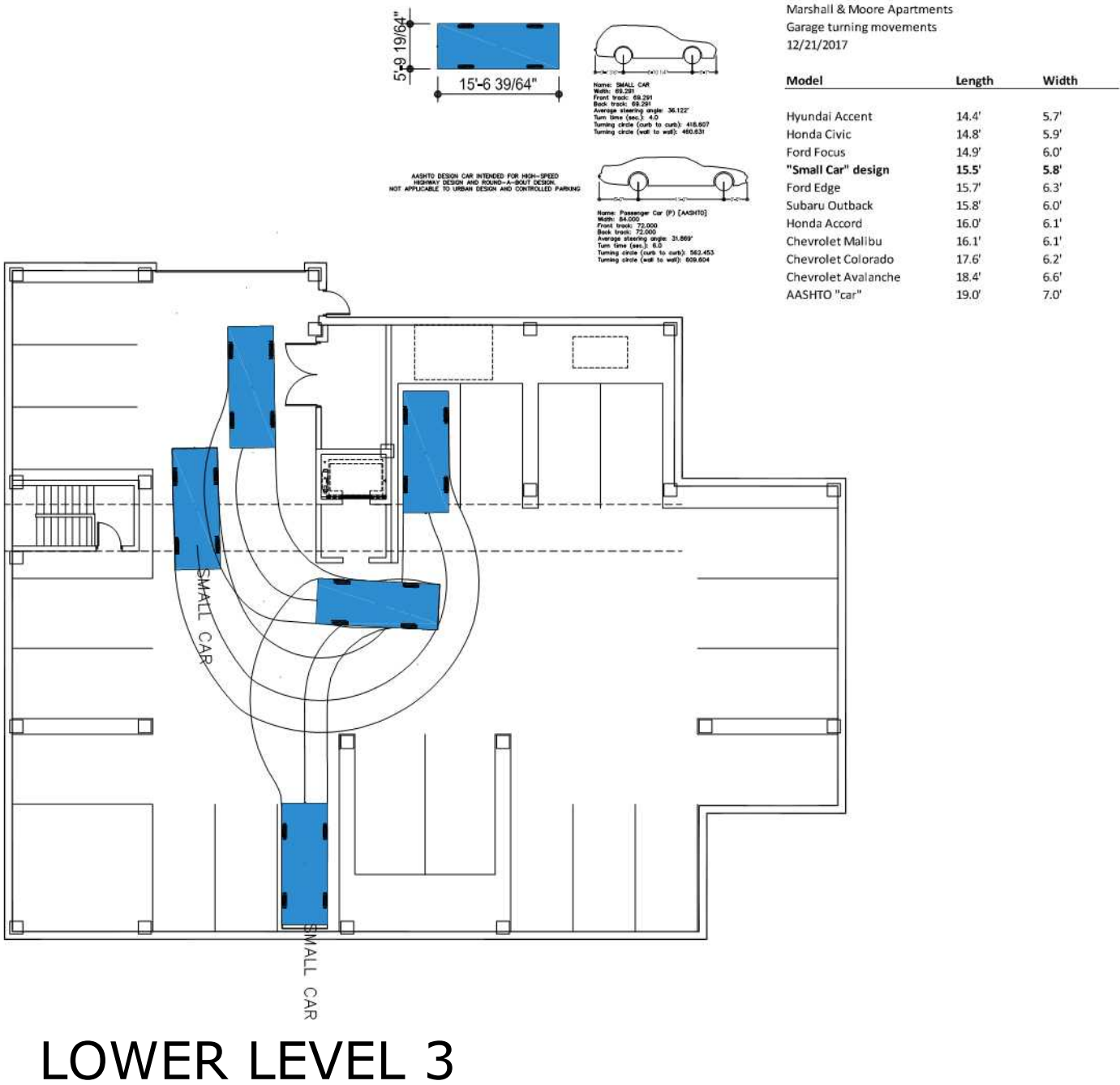


LOWER LEVEL 2

VEHICLE TURNING DIAGRAMS

MCR Property Holdings, LLC
MARSHALL & MOORE APARTMENTS
SAINT PAUL, MINNESOTA

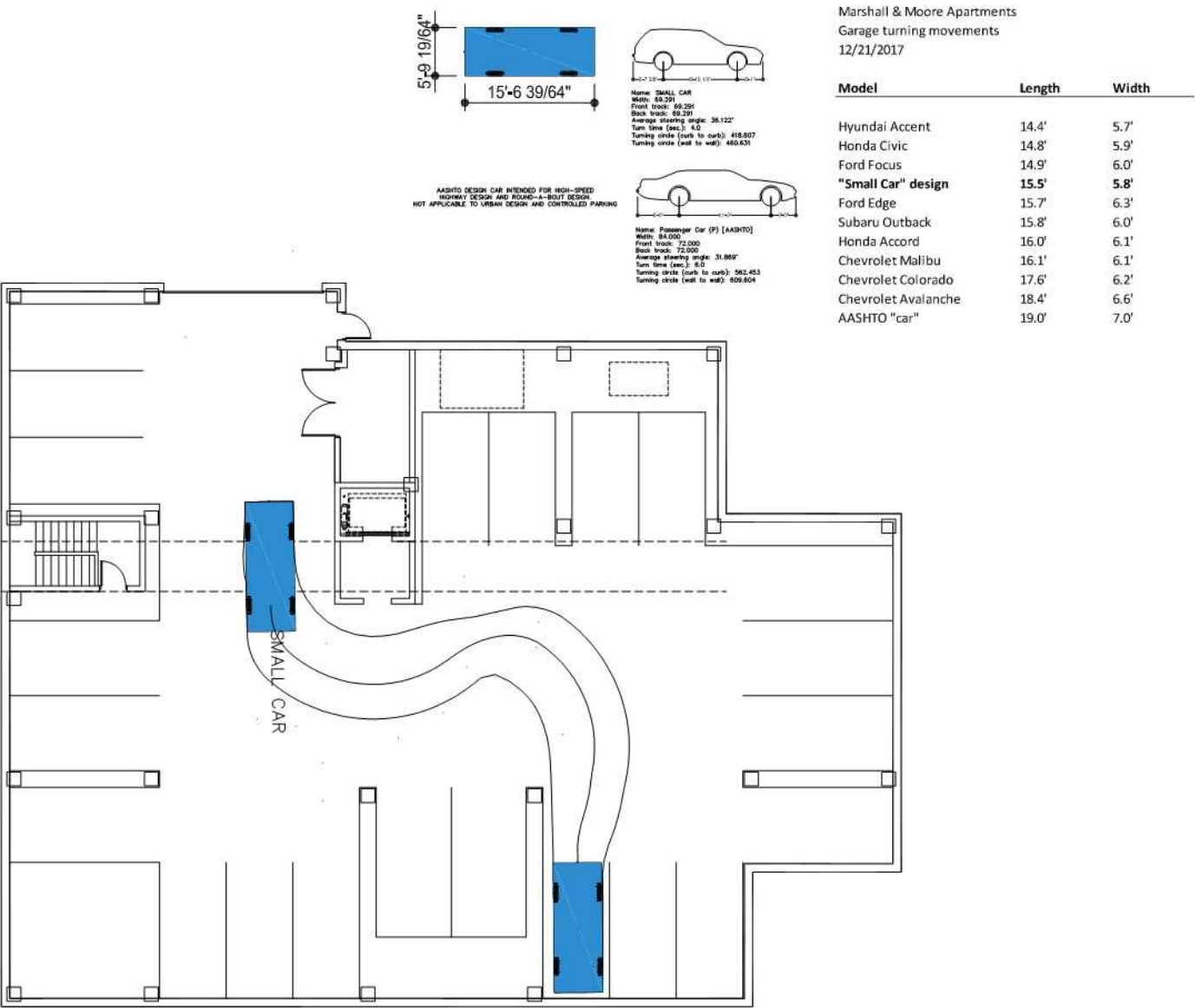
SITE PLAN REVIEW
12/28/2017
51666-17088



VEHICLE TURNING DIAGRAMS

MCR Property Holdings, LLC
MARSHALL & MOORE APARTMENTS
SAINT PAUL, MINNESOTA

SITE PLAN REVIEW
12/28/2017
51666-17088

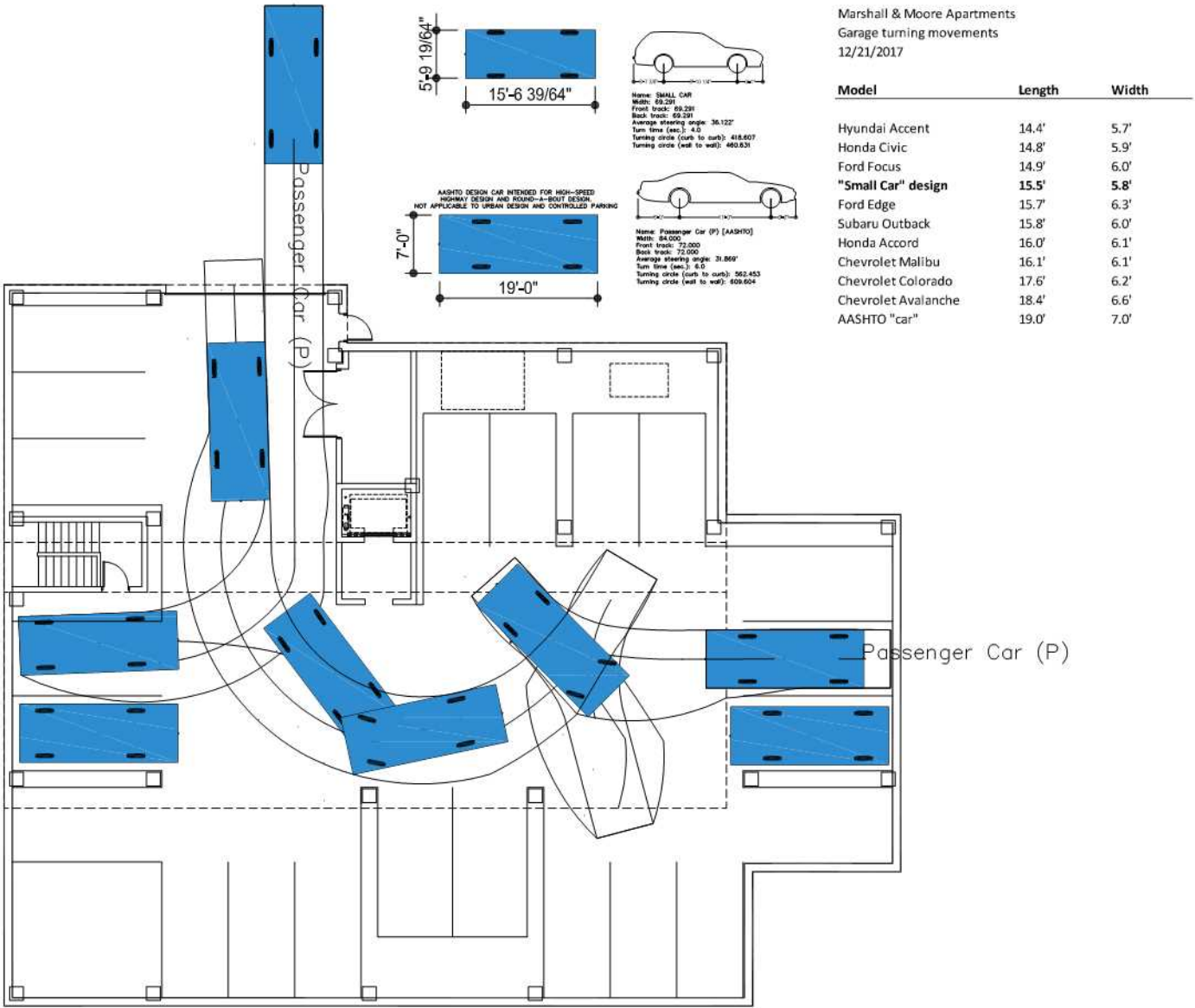


LOWER LEVEL 4

VEHICLE TURNING DIAGRAMS

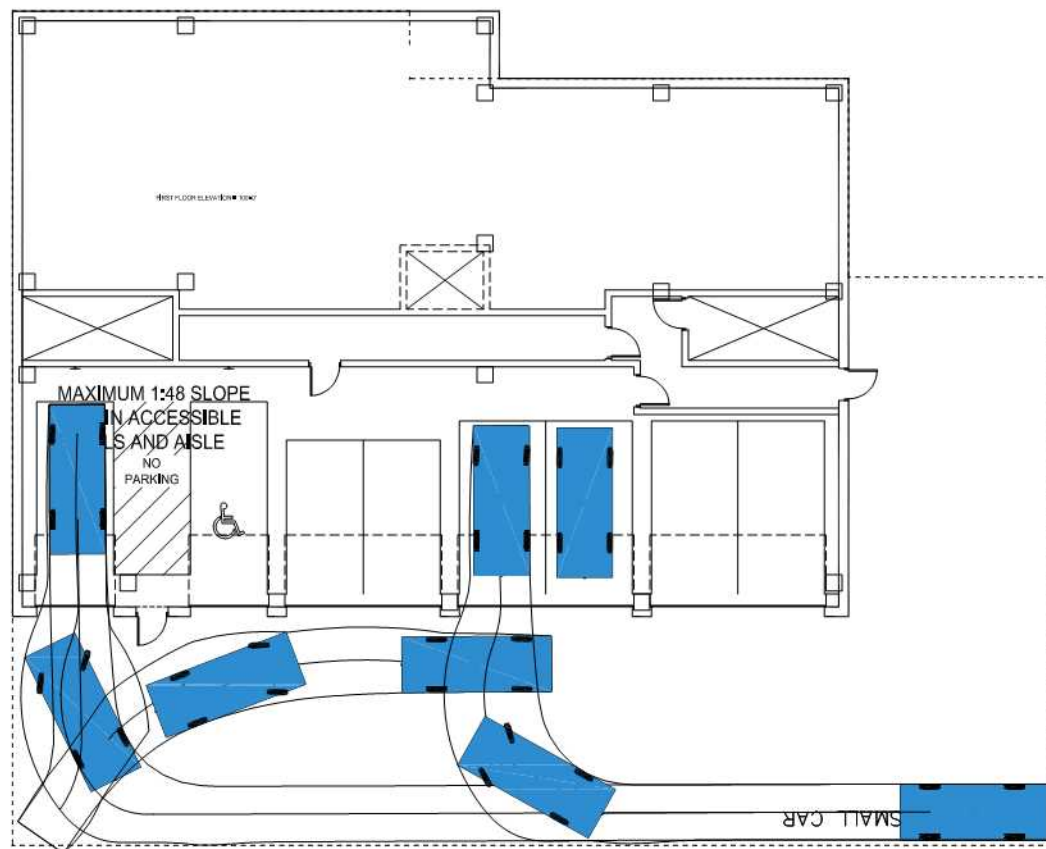
MCR Property Holdings, LLC
MARSHALL & MOORE APARTMENTS
SAINT PAUL, MINNESOTA

SITE PLAN REVIEW
12/28/2017
51666-17088

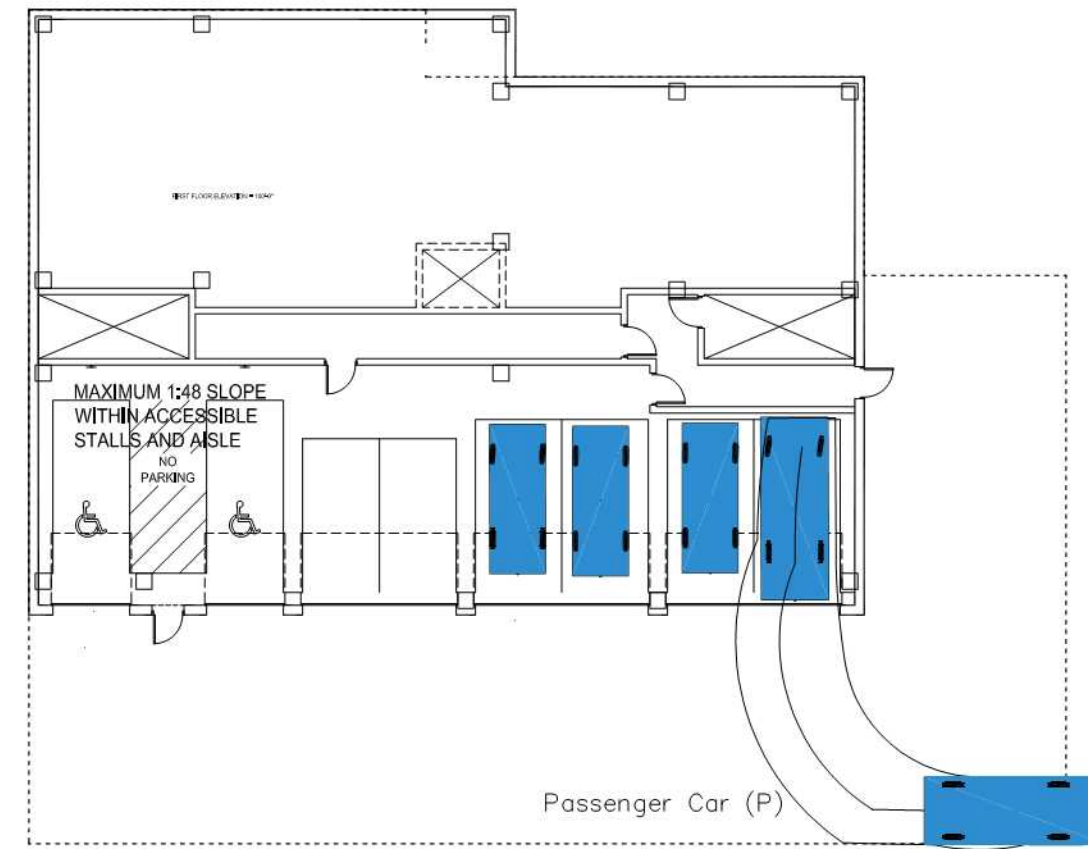


LOWER LEVEL 5

VEHICLE TURNING DIAGRAMS



UPPER LEVEL 1



UPPER LEVEL 2

MCR Property Holdings, LLC
MARSHALL & MOORE APARTMENTS
SAINT PAUL, MINNESOTA

SITE PLAN REVIEW
12/28/2017
51666-17088



VEHICLE TURNING DIAGRAMS



1. EXISTING



2. THREE STORY WITH 9 FT SETBACK



3. FIVE STORY WITH 28 FT SETBACK

JUNE 21



1. EXISTING



2. THREE STORY WITH 9 FT SETBACK



3. FIVE STORY WITH 28 FT SETBACK

JUNE 21



1. EXISTING

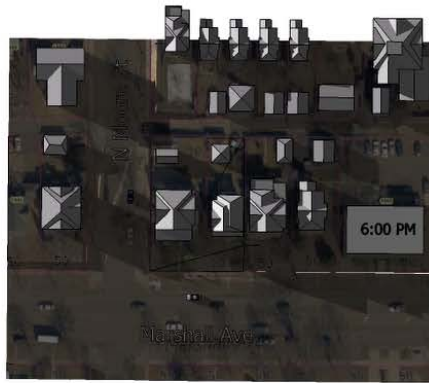


2. THREE STORY WITH 9 FT SETBACK



3. FIVE STORY WITH 28 FT SETBACK

JUNE 21



1. EXISTING



2. THREE STORY WITH 9 FT SETBACK



3. FIVE STORY WITH 28 FT SETBACK

JUNE 21



1. EXISTING



2. THREE STORY WITH 9 FT SETBACK



3. FIVE STORY WITH 28 FT SETBACK

SEPTEMBER 21



1. EXISTING



2. THREE STORY WITH 9 FT SETBACK



3. FIVE STORY WITH 28 FT SETBACK

SEPTEMBER 21



1. EXISTING



2. THREE STORY WITH 9 FT SETBACK



3. FIVE STORY WITH 28 FT SETBACK

SEPTEMBER 21



1. EXISTING



2. THREE STORY WITH 9 FT SETBACK



3. FIVE STORY WITH 28 FT SETBACK

SEPTEMBER 21

00:17







1. EXISTING

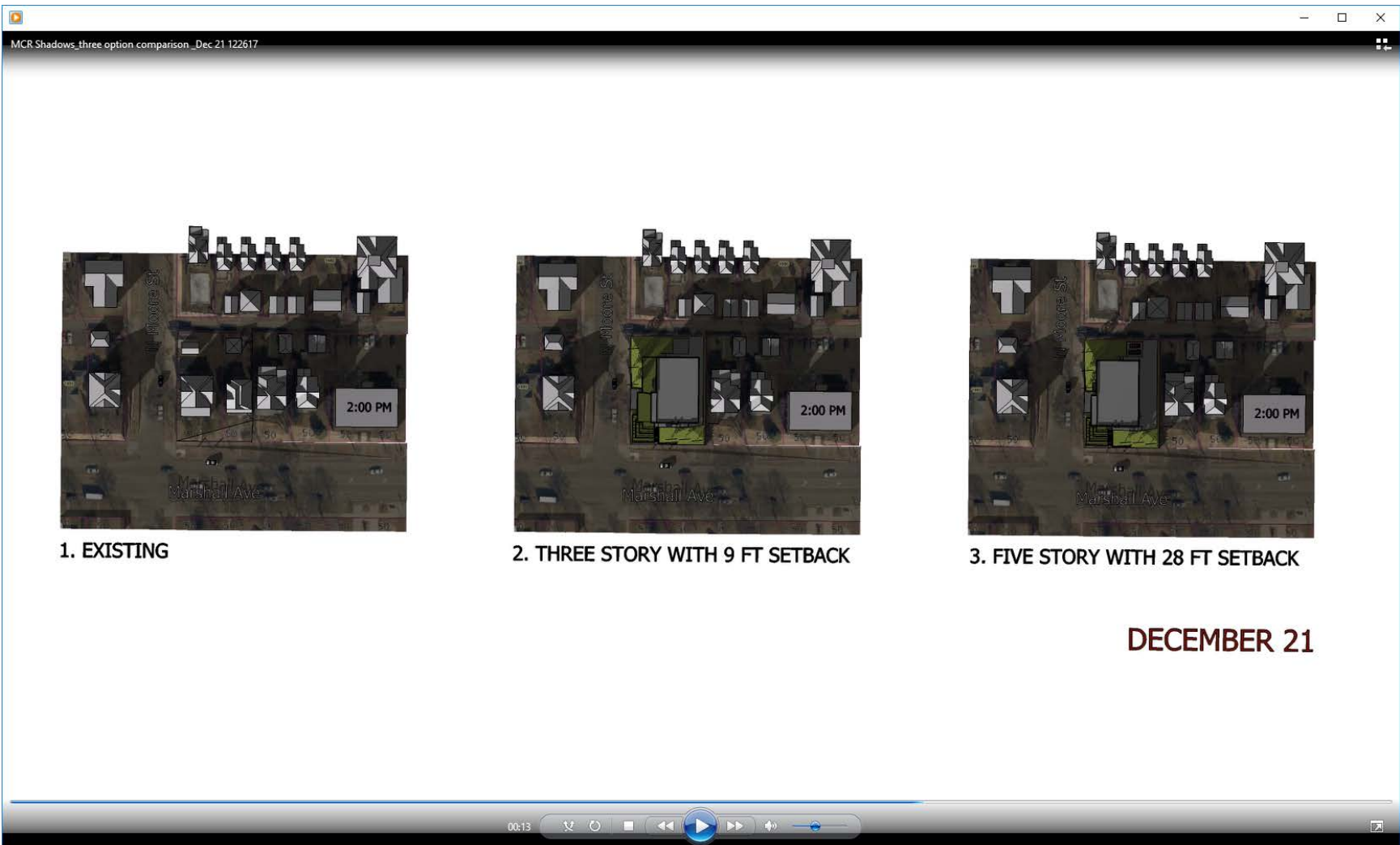


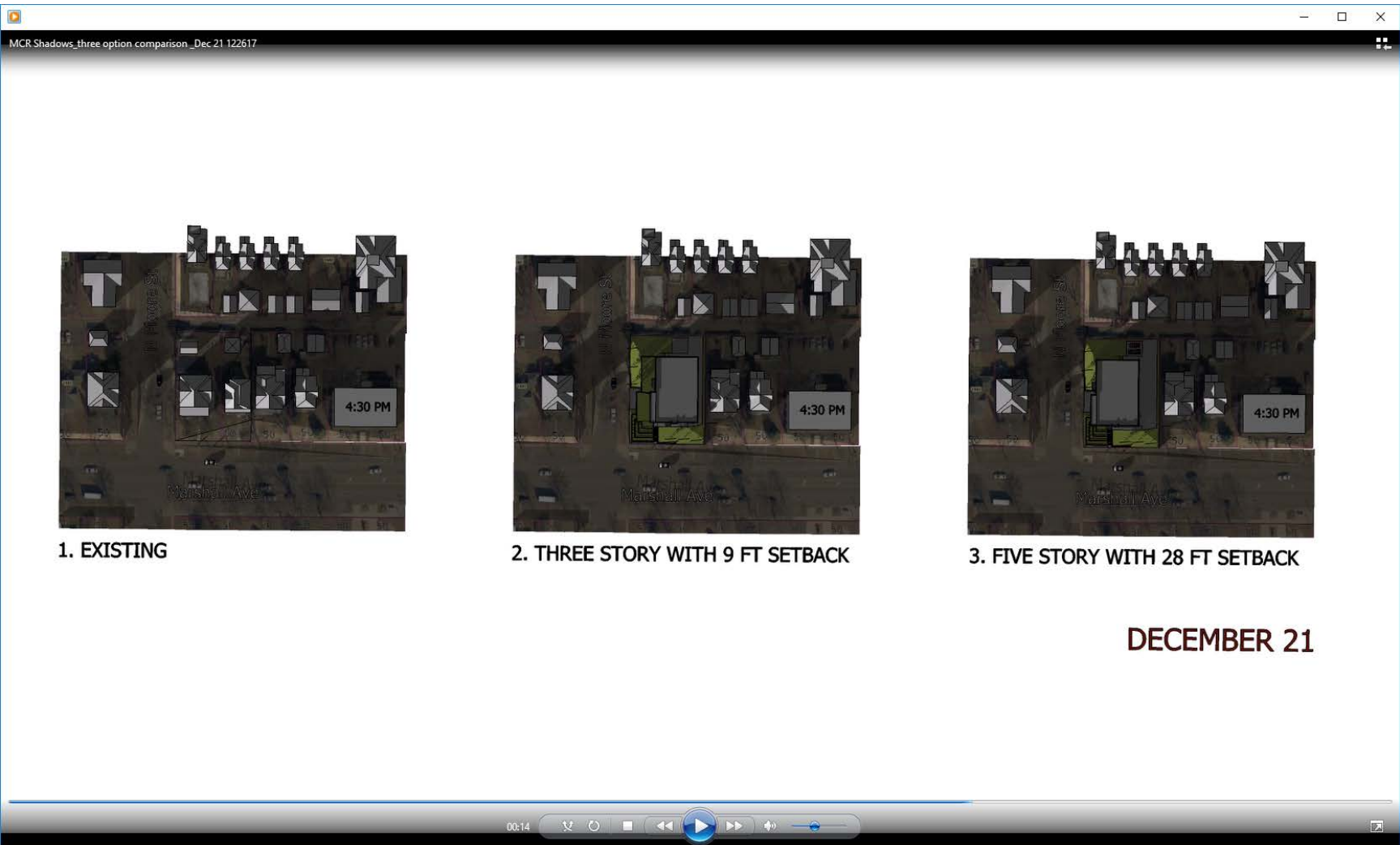
2. THREE STORY WITH 9 FT SETBACK



3. FIVE STORY WITH 28 FT SETBACK

DECEMBER 21





City of Saint Paul – Department of Safety and Inspections

Site Plan Review Report

Date of Report: November 9, 2017

SPR File # 17-206385

Address Location: 1973-1977 Marshall Ave

Project: Marshall & Moore Apartments



Jon Schwartzman
MCR Property Holdings, LLC
10921 Excelsior Blvd., #106
Hopkins, MN 55343

Paul Hanlon
Pope Architects
1295 Bandana Blvd., Ste 200
St Paul, MN 55108

Eric Meyers
Larson Engineering
3524 Labore Rd
White Bear Lake, MN 55110

On November 7, 2017, you met with City staff to discuss the site plan for the Marshall & Moore Apartment building project including 16 units and 32 off-street parking spaces. The comments from that meeting are summarized below.

1. Site Plan Process

- a) Site Plan Review is a function delegated by the St Paul Planning Commission to City staff, however, a Site Plan may be referred to Planning Commission for public hearing.
- b) For this project the overall Site Plan will receive a public hearing at the Zoning Committee of the Planning Commission. The public hearing date is to be determined. The Planning Commission shall determine whether the submitted site plan is approved or denied per the findings in Leg. Code Sec. 61.402. - Site plan review by the planning commission (c) site plan review and approval.
- c) Planning Commission approval of the Site Plan must be obtained before staff can sign-off on the Site Plan.
- d) A Final Site Plan decision by the Planning Commission may be appealed within ten days after the date of the decision per Leg. Code Sec. 61.702 – Appeals to city council.
- e) Provide a pdf version of the updated Site Plan package for review by the Site Plan Review Committee prior to submittal to the Planning Commission.
- f) Per Minnesota State Statute 326, the final plans submitted shall be signed by the appropriate licensed Professional, i.e. PE, LA, RLS, etc., responsible for plan development.
- g) Building permits will not be issued until the Site Plan has final approval.

2. Zoning

Reviewer: Tia Anderson/651-266-9086 tia.anderson@ci.stpaul.mn.us

Reviewer: Larry Zangs/651-266-9082 larry.zangs@ci.stpaul.mn.us

Comments:

- a) A development moratorium was approved by the City Council on October 18, 2017, on Marshall Avenue between Wheeler and Wilder Streets within the West Marshall Study Area. This project is within the moratorium area, however, a complete Site Plan application was submitted prior to the City Council public hearing on October 18, 2017.
- b) The proposed use of the property as Multi-family Housing is permitted at this location in a RM2 medium-density multiple-family residential Zoning District.
- c) *Please provide a description of the proposed office operations.* A leasing/maintenance office space serving the multi-family dwelling in which it is located would be considered an accessory use, which is permitted in an RM2 zoning district. However, an office serving multiple residential properties is a general commercial office use, and not permitted in any residential Zoning Districts.
- d) Tenants of multi-family residential units cannot exceed the definition of a family per Leg. Code Sec. 60.207. Family, which does not allow more than four un-related adults.
- e) RM2 setback requirements are as follows:
 - Minimum front setback is 29' 6" based on the average setback for the block.
 - Minimum side setback requirement is 9'.
 - Minimum rear setback requirement is 25'.

- f) RM2 Density standards are as follows:
 - Minimum lot area of 9,000 square feet is required for a multiple-family dwelling with 3 or more dwelling units.
 - One-half the width of a dedicated public alley adjoining the lot shall be considered as part of the lot, for the purpose of applying lot area and density requirements.
 - Minimum lot area per residential unit is 1,500 square feet. However, the lot area figure may be increased by three hundred (300) square feet for each parking space (up to two parking spaces per unit) within a multiple-family structure or otherwise completely underground.
 - The proposed accessory residential garages shall not be included in the lot area bonus calculation nor tandem spaces or other stacked parking.
- g) RM2 Dimensional standards are as follows:
 - *Update the Site Plan with the existing average grade based on the surveyed elevations.* Grade is the elevation established for the purpose of regulating the number of stories and the height of buildings. "Grade" shall be the mean level of the finished surface of the ground adjacent to the exterior walls of the buildings.
 - Maximum building height is 5 stories and 50', as measured from the established grade to the highest point of the roof surface for flat roofs.
 - Where a building is located on sloping terrain (e.g., Moore St elevation), the height may be measured from the average ground level of the grade at the building wall.
 - The existing grade of the property shall not be raised around a new building or foundation in order to comply with the height requirements of this code.
 - The height limitations of this code shall not apply to mechanical service stacks, tanks, ventilation equipment, and similar equipment.
 - *Update the Site Plan to clearly depict the building height and proposed elevations* in relation to the proposed structure in order to clarify average grade and height from all elevations.
- h) *Update the Site Plan with the lot coverage percentage.* In residential districts, principal buildings shall not cover more than thirty-five (35) percent of any zoning lot. The lot area may include One-half the width of the dedicated public alley adjoining the lot.
 - The green roof area exposed on the Moore Street side of the structure, shall be included in the lot coverage calculation.
 - Balconies shall be subject to setback and lot coverage requirements. However, per Leg. Code Sec. 63.105., an uncovered porch, deck or patio not exceeding two (2) feet in height is considered landscaping and is not subject to setback or lot coverage requirements.
- i) Attached vestibules or enclosed entrances may project up to twenty (20) square feet into a required front or rear yard and shall be included as part of the principal structure for lot coverage purposes.

3. Off-street Parking

- a) *Update the parking level plan with typical stall dimensions, maneuvering lane width, and number of parking spaces.* Workable parking spaces in the structured parking may be used for calculating the lot area density bonus.
- b) *Update the Site Plan analysis with number of residential units and bedrooms, and vehicle and bicycle parking required and provided.* The off-street parking requirement is based on number of residential units and number of bedrooms. 1.5 off-street parking space is required for 2 – 3 bedroom units, and 2 spaces for 4 bedroom units.
 - For dwelling units, a minimum of one secure bicycle parking space shall be provided for every 14 dwelling units.
 - Bicycle parking provided in excess of the minimum required bicycle parking may be substituted for up to 10 percent of minimum off-street vehicle parking requirements. For the purpose of calculating a substitution, 4 spaces in secure bicycle racks are the equivalent of one vehicle parking space.
- c) *Confirm location of secure bicycle parking.* Bicycle parking shall be provided in a convenient, safe, and secure location. The site plan included 16 bike racks in the rear of the property. However, at the November 7, 2017, Site Plan Review Committee meeting the project indicated they were considering bicycle parking in the underground parking area.
 - Outdoor bicycle parking shall be visible from the public right-of-way or from inside the building.

- Where motor vehicle parking spaces are monitored, covered or weather protected, required bicycle parking spaces shall be provided on the same basis.
- d) One ADA parking space is required for every 25 standard parking spaces. Parking spaces and passenger loading zones for persons with disabilities shall be designed in accordance with the provisions of the Accessibility Guidelines for Buildings and Facilities of the Americans with Disabilities Act (ADA).
- e) Accessory parking facilities may designate up to 50 percent of the spaces for compact cars. Compact spaces shall be designated by signs with a minimum of one sign per every four compact spaces. The submitted site plan currently includes 15 compact parking spaces.
- f) *Update the Site Plan to adhere to Zoning Code requirements for accessory structures* specifically as it pertains to the two proposed garages. Accessory buildings, structures or uses shall not be erected in or established in a required yard except a rear yard.
 - On corner lots, accessory structures shall be set back from the street a distance equal to that required of the principal structure.
 - When an accessory building, structure or use is constructed in a rear yard which adjoins a side yard or front yard, the accessory building, structure or use shall be set back from the interior lot line a distance equal to the minimum side yard required of the principal structure.
 - RM2 residential districts, accessory buildings on a zoning lot with residential use shall not exceed fifteen (15) feet in height, measured to the mid-point of the gable; provided, however, that accessory buildings with a flat or shed roof style shall not exceed twelve (12) feet in height.
 - Accessory buildings on a zoning lot may occupy up to thirty-five (35) percent of the rear yard. Rear yards which adjoin alleys may include half the area of the alley to calculate the area of the rear yard which may be occupied by accessory buildings.
 - An accessory building shall not be closer than one foot to an alley right-of-way.
 - Accessory buildings shall be located at least six (6) feet from the principal structure or shall be considered attached for purposes of the zoning code.
- g) *Update the Site Plan with dimensions and setbacks for the proposed off-street surface parking.* Off-street parking spaces shall not be within a required front or side yard and shall be a minimum of 4 feet from any lot line.
- h) At the November 7, 2017, Site Plan Review Committee meeting the project indicated that trash and recycling will be wheeled to the curb for pick-up.

4. Building Design Standards

- a) *Update the site plan with the dimensions of the decorative overhangs.* Overhangs and decorative details may project into a required yard sixteen (16) inches plus two (2) inches for each foot of width of the required side yard.
- b) *Update the site plan with location of the service door* to the structured parking in order to meet egress requirements within the parking level.
- c) The project shall comply with building design standards per Leg. Code Sec. 63.110:
 - A primary entrance of principal structures shall be located within the front third of the structure; be delineated with elements such as roof overhangs, recessed entries, landscaping, or similar design features; and have a direct pedestrian connection to the street.
 - Building materials and architectural treatments used on sides of buildings facing an abutting public street should be similar to those used on principal facades.
 - *Provide the percentage of window and door openings on new facades on the Site Plan.*
 - For principal buildings, above grade window and door openings shall comprise at least fifteen (15) percent of the total area of exterior walls facing a public street or sidewalk.
 - In addition, for new principal residential buildings, above grade window and door openings shall comprise at least ten (10) percent of the total area of all exterior walls.
 - Windows shall be clear or translucent.
 - The visual impact of rooftop equipment shall be reduced through such means as location, screening, or integration into the roof design. Screening shall be of durable, permanent materials that are compatible with the primary building materials. Exterior mechanical equipment such as ductwork shall not be located on primary building facades.

5. Lighting

- a) Exterior lighting shall meet Zoning Code Sec. 63.116. - Exterior lighting.
- All outdoor lighting shall be shielded to reduce glare and shall be so arranged as to reflect lights away from all adjacent residential districts or adjacent residences in such a way as not to exceed three (3) footcandles measured at the residence district boundary.
 - All lighting in all districts used for the external illumination of buildings shall be placed and shielded so as not to interfere with the vision of persons on adjacent highways or adjacent property.

6. Landscaping for the Site and Exterior Parking Lot

- a) All required yards and any underdeveloped space shall be landscaped using materials such as trees, shrubs, sod, groundcover plants, or stormwater landscaping.
- b) For off-street parking facilities that abut a residential use or zoning district across an alley, one (1) of the following shall be provided and maintained as determined by the zoning administrator as part of site plan review:
- A visual screen 4.5 – 6.5' in height; or
 - An ornamental metal fence or other non-screening, durable fence where security concerns make this preferable to a visual screen.
- c) For any parking facility, landscaping shall be provided to buffer the facility from adjacent properties and from the public right-of-way; reduce the visual glare and heat effects of large expanses of pavement; and provide areas for the retention and absorption of stormwater runoff. The standards can be found in Sec. 63.313 and 63.314 of the Zoning Code.
- *Perimeter Landscape* - A landscaped yard at least four (4) feet wide along the public street or sidewalk.
 - *Screening Landscape* shall be provided consisting of a masonry wall or decorative fence (not including chain link) between 3' and 4.5' tall along public street or sidewalk supplemented with a landscape buffer.
 - *Tree plantings* – At least 1 shade tree shall be planted for every 5 surface parking spaces.

7. Parkland Dedication

As a part of this project, the City requires payment of a Parkland Dedication fee (*in lieu of the property owner needing to dedicate a portion of the property as park land*). The fee collected is used by the City for acquiring park land or other park capital improvements in the area.

Staff estimates a dedication fee of \$1689. The fee is calculated based on the number of residential units proposed, number of existing units, and value of the property. Payment is included with building and inspection fees when the building permit is issued.

8. Signs

Reviewer: Ashley Skarda/651-266-9013 ashley.skarda@ci.stpaul.mn.us

Comments:

Business signs require a separate review and Sign Permit from the Department of Safety and Inspections. Site plan approval does not constitute approval of signs shown on the site plan. Contact Ashley Skarda of DSI Zoning regarding signs.

9. Planning

Reviewer: Kady Dadlez/651-266-6619 kady.dadlez@ci.stpaul.mn.us

Comments:

- a) The applicant's property is zoned RM2. An apartment building is a permitted use in the RM2 zoning district.
- b) This stretch of Marshall Avenue is identified as a Residential Corridor in the Land Use Plan, a chapter of the Saint Paul Comprehensive Plan. Residential corridors are described in the plan as segments of street corridors that run through Established Neighborhoods and are predominantly characterized by medium density uses (4 to 30 units per acre). The land use plan adds that some portions of Residential Corridors could support additional housing.

- c) The lot area of the property is 15,169 square feet (14,171 square feet plus half of the alley, 998 square feet) or .35 acres. Using the ratio of 30 units per acre, .35 acres would yield 10.5 units. The applicant plans to obtain a density bonus by providing 25 structured parking spaces (25 spaces x 300 square feet), thereby increasing the lot area for density consideration purposes by 7,500 square feet for a total lot area of 22,669 square feet or .52 acres. Using the ratio of 30 units per acre, .52 acres would yield 15.6 units. Sixteen units are proposed.
- d) Generally speaking the comprehensive plan calls for increased density in residential corridors consistent with the prevailing character and overall density of the area. The Union Park Community Plan, an addendum to the comprehensive plan, calls for finding a balance that preserves desirable assets and neighborhood character while evolving to meet present and future needs. Please refer to the attached document for specific references to land use and housing policies of the comprehensive plan and Union Park Community Plan.

10. District Council

The site is located in the District 13 Community Council. A copy of the site plan was provided to the District Council for comments. A letter from the District Council dated November 6, 2017, is attached for the project's consideration.

11. Public Works Records and Mapping

Contact Number: 651-266-6150

Comments:

- a) Provide proof of filing of combination of contiguous lots with Ramsey County.
- b) Contact Public Works Records and Mapping at 651.266.6150 to determine the address for the building and apartment addresses for the individual units within the building. The building address can be odd numbers within the range of 1973 Marshall to 1979 Marshall.

12. Public Works Construction

Reviewer: Scott Brimer/651-266-6225 scott.brimer@ci.stpaul.mn.us

Comments:

Curb at alley entrance must not be placed into alley right of way, but terminate at the property line. Match existing bituminous at alley.

13. Public Works Transportation Planning

Reviewer: David Kuebler/651-266-6217

david.kuebler@ci.stpaul.mn.us

Reviewer: Colleen Paavola/651/266-6104

colleen.paavola@ci.stpaul.mn.us

Comments:

- a) Please be advised that a Temporary Pedestrian Access Route (TPAR) and/or a Temporary Traffic Control (TTC) plan may be required as part of the Right-of-Way (ROW) permitting process. Said TTC or TPAR plans must be approved by the City prior to the ROW Permitting office issuing a permit(s).
- b) Per Minnesota State Statute 326, the final plans submitted must be signed by the appropriate licensed Professional, i.e. PE, LA, PLS, etc., responsible for plan development.
- c) On Plan Sheet S1 please include existing boulevard infrastructure, i.e. signs, lighting, etc. Also show the existing lane striping on Marshall.
- d) On Plan Sheet C1 please include notes regarding the protection of, or impacts to, the existing boulevard infrastructure such as street lighting, signs, etc.
- e) Please provide turning templates relative to accessing and exiting the proposed three stall garage, the two on-grade stalls and the two stalls in the garage as shown on Plan Sheet C2.
- f) Please provide detailed dimensions to Plan Sheet C2. The width dimension of the driveway at the ROW line is to be 22 feet unless turning movements indicate something wider is needed.
- g) On Plan Sheet C3.0 please provide a detail of the sidewalk/ramp relationship between ADA compliant ramp that starts at Marshall and parallels the Moore mainline sidewalk.
- h) On Plan Sheet C3.0 please provide catchbasin erosion control devices for the catchbasin at the NE quadrant of the Moore/Marshall intersection.

- i) On Plan Sheet C5 please update the detail for the driveway apron to a 1206D as can be provided by the City's Sidewalk office. Contact Al Czaia for said detail.
- j) For the plan sheets completed by Pope Architects, please include the ROW line in the elevation views and verify that what is shown in the elevation views represents what is shown in the Civil plan sheets. For example, the west elevation as provided is different than what is shown on plan sheet C3.0 in that the ADA ramp is not shown and the location of the walk-in door is on the wrong side of the overhead door.
- k) For the plan sheets completed by Pope Architects, the Garage Plan needs more detail such as grades, drainage information, turning movements showing how vehicles are able to negotiate the area, etc.
- l) Update the Site Plan with the following notes:
 - Change the NSPECTION CONTACT to "Dick Rohland at 651.485.1688".
 - As part of the ROW permitting process, two weeks before any work begins that impacts the ROW in any way the developer shall provide to the ROW Inspector the name and contact information of the Construction Project Manager or Construction Project Superintendent. If this information is not provided there may be a delay in obtaining permits for the work in the ROW. Said delays will be the sole responsibility of the developer
 - Replace the "ENCROACHMENT" note with the following:
"ENCROACHMENTS: Per Chapter 134 of the Legislative Code, no person shall construct and maintain any projection or encroachment within the public right-of-way. Construction of the development that necessitates temporary use of the Right-of-Way (ROW) for construction purposes shall be limited to equipment, personnel, devices and appurtenances that are removable following construction. Encroachment permits will not be granted for devices such as tie backs, rock bolts, H-piles, lagging, timbers, sheet piling, etc. that the owner is seeking to abandon in the ROW.
Section 3201.3 of the Minnesota Building Code defers final authority of encroachments into public rights-of-way/public property to the local authority. City Legislative Code governs management of the public rights-of-way. Provided such installations are approved by Public Works, footings may be allowed to encroach into City ROW no more than twelve (12) inches at depths below eight (8) feet as provided for in Minnesota Building Code Section 3202.1. Said encroachments would require an encroachment permit from the City per Chapter 134 of the Legislative Code.
Encroachments into County or State ROW are not allowed unless authorization has been granted from said agency. Marshall Avenue is a County facility.
 - Change the contact name in the "NO PRIVATE FACILITIES IN THE RIGHT OF WAY" note to Don Bjorkman.
 - Access to signal controller and lighting cabinets must be maintained at all times. If fencing is required for a job site, a key or other means of access must be provided to the City of St. Paul's Traffic Operations Department. Contact John McNamara, General Foreman Signals and Lighting at 651.266.9780 for more information.
 - The paragraph that begins "As per the City's "Standard Specification for Street Openings" needs to be titled "ROADWAY RESTORATION".
 - Please include the following notes on the plans:
 - SIGNING: Signs regulating parking and/or traffic on private property shall be installed by the property owner or contractor outside of the public right-of-way (ROW). Removal of signs within the public ROW shall be completed by the City. New signs or the reinstallation of existing signs, as approved by Public Works Traffic Engineering, regulating parking and/or traffic in the public ROW for this development shall be installed by the City at the expense of the development. Contact Chris Gulden of Public Works 651-266-9778 two weeks in advance of needed sign work.
 - METERS: The City shall be contacted if development work affects the parking meter system. Said system includes meters, space numbers, pay stations and all appurtenances needed for the system to be operational. Removal of meter system elements within the public ROW shall be completed by the City and charged to the

developer. The installation of new meters or the reinstallation of removed meters shall also be completed by the City and charged back to the developer. The developer/contractor shall contact Chris Gulden at 651.266.9778 two weeks in advance of needed meter work. If two weeks' notice is not provided there may be a delay in obtaining permits for the work in the ROW. Said delays will be the sole responsibility of the developer.

m) CITY OF ST. PAUL PERMIT REQUIREMENTS:

- ORDERING OBSTRUCTION AND EXCAVATION PERMITS: Contact Public Works Right of Way Service Desk at (651) 266-6151. It is strongly recommended that contractors call for cost estimates prior to bidding to obtain accurate cost estimates.
- OBSTRUCTION PERMITS: The contractor must obtain an Obstruction Permit if construction (including silt fences) will block City streets, sidewalks or alleys, or if driving over curbs.
- EXCAVATION PERMITS: All digging in the public right of way requires an Excavation Permit. If the proposed building is close to the right of way, and excavating into the right of way is needed to facilitate construction, contact the utility inspector.
- FAILURE TO SECURE PERMITS: Failure to secure Obstruction Permits or Excavation Permits will result in a double-permit fee and other fees required under City of St. Paul Legislative Codes.

14. **Public Works Sidewalks**

Reviewer: Al Czaia/651-266-6108

al.czaia@ci.stpaul.mn.us

Comments:

- a) Contractor is responsible for damage to the mainline sidewalk, curb, drive access and boulevard landscaping cause during the construction. Contractor advised to document pre-existing condition of the right of way prior to commencement of the construction.
- b) Sidewalk grades must be carried across driveways.
- c) Update the Site Plan with the following notes:
 - CONSTRUCTION IN RIGHT OF WAY: All work on curbs, driveways, and sidewalks within the public right of way must be done to City Standards and Specifications by a contractor licensed to work in the City right-of-way under a permit from Public Works Sidewalk Section (651-266-6108). Sidewalk grades must be carried across driveways.
 - RIGHT OF WAY RESTORATION: Restoration of asphalt and concrete pavements are performed by the Public Works Street Maintenance Division. The contractor is responsible for payment to the City for the cost of these restorations. The contractor shall contact Public Works Street Maintenance to set up a work order prior to beginning any removals in the street at 651-266-9700. Procedures and unit costs are found in Street Maintenance's "General Requirements - All Restorations" and are available at the permit office.

15. **Ramsey County**

Reviewer: Erin Laberee/651-266-7105 erin.laberee@co.ramsey.mn.us

Comments:

A copy of the Site Plan was provided to Ramsey County for review. Ramsey County has no comments on the plan as submitted.

16. **Metro Transit**

Reviewer: Berry Farrington/612-349-7378 berry.farrington@metrotransit.org

Comments:

- a) The plans show the developer is responsible for constructing a replacement boarding pad for the bus stop. At minimum it must be 5 feet wide by 8 feet deep and meet the standards identified by the Americans with Disabilities Act of 1990.
- b) The retaining wall is adjacent to a bus stop and the first level of the wall may be used as seating. Metro Transit suggests the wall design include surface for seating which will help protect the landscaping from damage.

- c) Please coordinate any needed short term lane closures with Metro Transit Street Operations, Greg Tuveson (612-349-5407 / greg.tuveson@metrotransit.org)

17. Public Works Sewers

Reviewer: Anca Sima/651-266-6237 anca.sima@ci.stpaul.mn.us

Comments:

- a) Add a note on the demolition plan: removal permit for A11882 & 12719: cut off at the property line.
- b) Provide the detailed drainage map and the HydroCad calculations (some roof ponding)
- c) The EOF should be at least 1 ft. lower than FFE. Show the EOF on the plan.
- d) If the existing manhole for the proposed sanitary service connection is a brick construction, reconstruction of the manhole access may be required, please add the attached detail (2322e) for manhole reconstruction.
- e) The proposed man hole should be further than 50ft away from the existing one, if not, connect the proposed connection into an existing one.
- f) Add the city's standard plate with the connection at the main.
- g) If the storm pipe is less of 5" of building foundation, than is under the plumbing jurisdiction. It should be a steel pipe and you have to provide 5 pound air test.
- h) Show that all the storm water will be captured in the trench for 100 years event.
- i) Sanitary and/or storm sewer service passing within 10 feet of the building are governed by the MN Plumbing Code. Specification for pipe material selection and notes for required air test of the piping, compliant with the MN State Plumbing Code, must be shown on the plan.
- j) Update the Site Plan with the following notes:
 - SEWER REPAIR PERMIT: Plumbing Contractor to obtain "Repair Permits" from Public Works for proposed modification to the existing storm sewer connections. Call St Paul PW permit desk (651-266-6234) for information on obtaining this permit.
 - SEWER REMOVAL/ABANDONMENT PERMIT: Plumbing Contractor to obtain "Removal Permits" from Public Works to cut off existing sewer connections services to the property. Call St Paul PW permit desk (651-266-6234) for information on obtaining this permit.
 - SEWER CONNECTION PERMIT: License house drain contractor to obtain (Sewer Connection Permit) to construct new sanitary and storm connection in street from main to the property. Call St Paul PW permit desk (651-266-6234) for information on obtaining this permit.

18. Water Quality/Erosion Control

Reviewer: Wes Saunders-Pearce/651-266-9112 wes.saunders-pearce@ci.stpaul.mn.us

Comments:

- a) Erosion control plan must show silt fence / perimeter controls within the private property not the public boulevard.
- b) Erosion control plan must add inlet protection to catch basins on Moore and Marshall.
- c) Provide more details on green roof including specific landscaping proposed and design cross section. Consider provisions for signage or fencing to prevent inadvertent impact by occupants.
- d) There must be 1 foot of separation from the 100-yr high water level and low floor.

19. Water Utility

Reviewer: Jeff Murphy/ 651-266-6813 jeffrey.murphy@ci.stpaul.mn.us

Comments:

- a) Show existing water services on Plan Sheet C1 or C4 to be cut off at the water main.
 - 1977 Marshall Ave – 1" CP – 30 S N Marshall / 28 E E Moore
 - 1973 Marshall Ave – 1" CP – 80 E E Moore
- b) The following work shall be performed by SPRWS on an actual cost basis. An estimate will be provided and payment in the amount of the estimate must be received before the work can be scheduled. Work of this type is currently being scheduled 4 to 6 weeks after payment and required signatures have been received:

- Pipework within right of way for 4" ductile iron domestic service.
 - Pipework within right of way for 6" ductile iron fire service.
 - Cut off of existing unused water services at the main.
 - Inspection of water facility work performed by owner's contractor.
- c) Plumbing permit applications to be made with SPRWS at 1900 Rice Street, Saint Paul, MN.
- d) Before construction of a new water service can be scheduled, SPRWS must receive a Water Service Contract signed by the owner and all required payments.
- e) Provide completed project data sheets to determine meter sizing.
- f) Furnish one set of interior fire suppression mechanical plans for review and approval by SPRWS plumbing inspection unit.
- g) Furnish one set of revised site plans for review. Following approval by SPRWS, furnish one set of approved plans.
- h) Provide the following on PLAN SHEET C4 under NOTES:
- Water services to be installed according to SPRWS "Standards for the Installation of Water Mains.
 - A four-sided trench box is required on all excavations deeper than 5 feet where underground work or inspection is to be performed by SPRWS. Ladders are required and must extend 3 feet above the surface of the trench. Sidewalks, pavements, ducts and appurtenant structures shall not be undermined unless a support system or another method of protection is provided. Trenches in excess of 20 feet in depth must be signed off by a registered professional engineer. Excavated material must be kept a minimum of 2 feet from the edge of the trench.
 - Pipe material for 8" Ductile Iron Pipe must be Class 52, Pipe material for 6" and 4" Ductile Iron Pipe must be Class 53. The exterior of ductile iron pipe shall be coated with a layer of arc-sprayed zinc per ISO 8179. The interior cement mortar lining shall be applied without asphalt seal coat.
 - Pipe must be wrapped in V-Bio Polywrap encasement.
 - Maintain 3 feet vertical separation between water and sewer pipes or a 12 inch separation with 4 inch high density insulation per SPRWS Standard Plate D-10 for typical water main offsets.
 - Refer to SPRWS "Standards for the Installation of Water Mains" Standard Plate D-11 for restrained pipe requirement.
 - All water service valve boxes within construction area must be exposed and brought to grade upon completion of construction.
 - All pipe work inside of property to be performed by a plumber licensed by the State of Minnesota and Certified by the City of Saint Paul. SPRWS requires separate outside and inside plumbing permits for each new water service.
 - All unused existing water services to be cut off by SPRWS. Excavation and restoration by owner's contractor. New water services will not be turned on until required cutoffs have been performed.
 - Water facility pipework within right of way to be installed by SPRWS. Excavation and restoration by owner's contractor.
 - The contractor providing excavation is responsible for obtaining all excavation and obstruction permits required by any governing authority.

20. Fire

Reviewer: Ann McLoughlin-651-266-9140

ann.mcloughlin@ci.stpaul.mn.us

Comments:

- a) Visibility of the Fire Department connection may be located on the N Moore St side of the building as close to Marshall Ave as possible in a visible, accessible location at all times without obstruction by fences, bushes, trees, walls, or other objects for a minimum of 3 feet to the front and each side. This requirement is applicable for the duration of the construction time as well.

- b) A metal sign or collar with raised letters at least 1 inch in size must be mounted on all Fire Department connections serving fire sprinklers, standpipes, or fire pump connections. Such signs shall read AUTOMATIC SPRINKLERS or STANDPIPES or TEST CONNECTION or a combination thereof as applicable.
- c) Automatic Fire Sprinkler System Required. Plans and permits required by licensed contractor. Flow test is required to determine adequacy of water service for proposed fire suppression system. The fire prevention section of the Dept. of Safety and Inspections reviews sprinkler plans, issues permits and does the necessary inspections of the installation. Contact Jeff Hemenway 651-266-8952 with questions on obtaining this permit and the procedure for arranging a time to perform this test.
- d) Update the Site Plan with the following notes:
 - Contractor to maintain access to the fire department connection for fire department personnel at all times during the construction period.

21. City Forestry

Reviewer: Zach Jorgensen/651-632-2437

zach.jorgensen@ci.stpaul.mn.us

Comments:

- a) Street tree removals:
 - Review utility placement and impacts to street trees on Marshall. If excavation for underground utility lines will be within 8ft of the trunk of a tree then removal and replacement will be required. Demo and landscape plans are to be updated accordingly. Contact me with questions.
- b) Update the landscape plan:
 - Identify the tree types to be planted. Street trees are to be swamp white oak or hackberry.
 - Include a plant schedule.
 - Trees planted on the boulevard are to be 2" caliper and have a minimum one year warranty.
- c) Ensure tree protection fencing is maintained throughout the construction process.
- d) Existing street trees are to be protected at all times. Trees damaged or removed during construction shall be restored or replaced to the satisfaction of, and at no cost to, the City as determined by the Forestry manager.
- e) Boulevard restoration is to include the following:
 - Where driveways, sidewalks or other surface paving are removed all concrete, asphalt and base materials shall be removed.
 - Boulevard soils are to be protected during construction. Soil compaction due to construction activities shall be mitigated and soils loosened prior to final grading.
 - Boulevards shall be restored with a minimum of 4" of topsoil.
- f) Concrete washouts shall not be located within the drip line of a tree.
- g) Update the Site Plan with the following notes (notes are to be included on the demo and landscape plan sheets):
 - The removal, pruning, and/or planting of trees on the public boulevard requires an approved permit from the City Forester (651-632-2437). Any work must be completed by a licensed tree contractor.
 - Construction supplies, materials, spoils, equipment, and vehicles shall not be stored or operated within the drip line of any public street tree or on turf boulevards without prior written approval from the City Forester. If the boulevard must be used for construction activities, site access routes, material storage or other related activities, protective measures approved by the City Forester shall be taken to reduce soil compaction and protect tree(s) from damage.
 - Street trees shall be protected by establishing a tree protection zone using 4' tall fencing installed at the drip line of the tree. Tree protection fencing shall be installed prior to the start of any site work and maintained for the duration of the project. Proposed work within, or changes to the location of tree protection fencing shall be reviewed by the City Forester prior to alteration.
 - The location of tree protection fences shall be shown on the plan.
 - A tree protection fence detail shall be included in the plan set (see attached).

- Contractor shall contact the City Forester (651-632-2437), prior to demolition or other land disturbance associated with site construction, to verify tree protection measures.
- Tree planting details shall include the following:
 - Expose root flare and set at grade.
 - Remove burlap and ropes from top 1/3rd of root ball, cut wire basket down to second horizontal wire from the bottom, and dispose of off-site.
 - Contractor is responsible to maintain trees in a plumb position throughout the maintenance period.

22. Parks and Recreation

Reviewer: Paul Sawyer/651-266-6417

paul.sawyer@ci.stpaul.mn.us

Comments:

No concerns

23. Plumbing

Reviewer: Rick Jacobs/651-266-9051 rick.jacobs@ci.stpaul.mn.us

Comments:

- Contact Saint Paul Regional Water Services (SPRWS) for questions, permits, fees, inspections, specifications, plans, or information that may be required for the water service and/or the water meter.
- No utilities must run under any accessory structures.
- The accessory buildings used to store or house vehicles must not have drains and the floors must be pitched toward the outside via the opening for overhead garage door.
- Sanitary and/or storm sewer service passing within 10 feet of the building are governed by the MN Plumbing Code. Specification for pipe material selection and notes for required air test of the piping, compliant with MN State Plumbing Code 4714 Section 1109.0, must be shown on the plan. This system must be reviewed and approved by Rick Jacobs, Senior Plumbing Inspector (651-266-9051) at the time of the Plumbing Plan review to ensure that it meets Plumbing Code standards.
- Contact the City of Saint Paul Department of Public Works Sewer Division for questions, permits, fees, inspections, specifications, plans, or information that may be required for sewer and storm piping work performed outside the building, including "retention systems".
- All primary roof drains shall be piped internally to below grade, exit the building, and be connected to the storm sewer. MPC 4714.1101.1.
- Secondary Roof Drainage shall drain to an approved place of disposal in the form of Secondary Roof Drains installed per MPC 4714.1101 & 1102, and Minnesota State Building Code 1503.4 1-5. Secondary roof drainage must discharge onto permeable soils and cannot drain onto the sidewalk. MPC 4714.1101.1. An approved place of disposal that can be approved by the Authority Having Jurisdiction for secondary roof drainage is in the form of secondary roof drains piped internally, down to within 18 inches of grade, through the outside wall, onto a splash block installed per MPC 1101.5.3, and laid over permeable soils of an adequate amount where saturation of the soil will not occur.
- The "green roof" design will require approval of an Alternate Code Compliance under MPC 4714 301.2 as it is not a design approved by the plumbing code. Provide a proposed design and completed DSI Alternate Code Compliance form to the DSI Plumbing Section for approval prior to site plan approval. All discharge to the "green roof" area must be included in the total drainage calculations of the green roof.

24. Building Code Requirements

Reviewer: James Williamette/651-266-9077 james.williamette@ci.stpaul.mn.us

Comments:

- This proposal will require a demolition permit to remove the existing structures and a building (grading) permit to proceed with any grading activity.
- This proposal will require a building permit to proceed. The building permit is issued only after all necessary city staff have approved and signed off on the proposed design. In addition to the building permit, separate permits are required for any plumbing, electrical and mechanical work,

elevator installation and any fire sprinkler modifications. These permits must be obtained and the work performed by city licensed contractors in each of the respective trades.

- c) One PDF and two sets of complete construction documents stamped by public works must be submitted with the building permit application to the DSI Main Office/ Permit Desk.
- The construction documents shall include architectural, structural, mechanical, electrical and plumbing plans signed and stamped by design professionals registered in the State of Minnesota. The architect shall provide a complete code analysis and a color-coded exit plan showing all fire rated walls and shafts and include exit access and travel distances. The plans shall have the energy code noted on the plans along with the compliance path chosen. The submittal should include compliance documents detailing how the energy code requirements are met.
 - Plans must be dimensioned, drawn to scale and sufficiently detailed to denote the scope of work to be performed and the method of construction.
 - Mechanical ventilation plans will need to be prepared by a mechanical engineer, registered with the State of Minnesota. In some cases, a "Master in the Trade" may prepare plans. The ventilation contractor should contact our senior warm air inspector Gary Reinsberg (651-266-9064) or by e-mail at Gary.reinsberg@ci.stpaul.mn.us The Energy code and path must be noted on these plans also.
 - The plumbing and electrical contractors for this project should contact our office if they have questions about whether engineered plans need to be submitted with their permit request. The senior plumbing inspector is Rick Jacobs at 651-266-9051 Rick.jacobs@ci.stpaul.mn.us and the senior electrical inspector is Dan Moynihan at 651-266-9036 Dan.monihan@ci.stpaul.mn.us

Note: **The building permit will not be issued until all signoffs are received** from Zoning, HPC, HVAC, Public Works, or other departments that are assigned to the project.

25. Service Availability Charge (SAC)

The proposed project will need a SAC determination before a building permit can be issued. You must submit a copy of the plans to the Metropolitan Council Environmental Services (MCES) for a SAC determination. Please see their website at <https://metro council.org> for additional information. If MCES determines that a SAC fee is due, the City will collect that payment with the building permit fee.

SAC Questions and Determination Review Submittal Information email:

SACprogram@metc.state.mn.us or call 651-602-1770 to speak to a SAC representative.

Report Prepared By:



Tia Anderson
Senior City Planner
Dept. of Safety and Inspection
375 Jackson St - Suite 220
Saint Paul MN. 55101-1806
651-266-9086
tia.anderson@ci.stpaul.mn.us

Cc: File, Site Plan Review Staff, Ramsey County, Metro Transit, City Council Ward 4 Office, District 13 Community Council

Marshall and Moore Apartments – 1973-1977 Marshall Avenue

Planning Comments: November 9, 2017

The applicant's property is zoned RM2. An apartment building is a permitted use in the RM2 zoning district. This stretch of Marshall Avenue is identified as a Residential Corridor in the Land Use Plan, a chapter of the Saint Paul Comprehensive Plan. Residential corridors are described in the plan as segments of street corridors that run through Established Neighborhoods and are predominantly characterized by medium density uses (4 to 30 units per acre). The land use plan adds that some portions of Residential Corridors could support additional housing.

The lot area of the property is 15,169 square feet (14,171 square feet plus half of the alley, 998 square feet) or .35 acres. Using the ratio of 30 units per acre, .35 acres would yield 10.5 units. The applicant plans to obtain a density bonus by providing 25 structured parking spaces (25 spaces x 300 square feet), thereby increasing the lot area for density consideration purposes by 7,500 square feet for a total lot area of 22,669 square feet or .52 acres. Using the ratio of 30 units per acre, .52 acres would yield 15.6 units. Sixteen units are proposed.

Generally speaking the comprehensive plan calls for increased density in residential corridors consistent with the prevailing character and overall density of the area. The Union Park Community Plan, an addendum to the comprehensive plan, calls for finding a balance that preserves desirable assets and neighborhood character while evolving to meet present and future needs. Please refer to the attached document for specific references to land use and housing chapters of the comprehensive plan and Union Park Community Plan.

Land use chapter of the comprehensive plan

Land Use Policy 1.1 states, this policy is intended for the development of housing in residential corridors consistent with the prevailing character and overall density of the area. The density goal is 4-30 dwelling units per acre in residential corridors.

The proposed site is located along an existing transit line and is less than a mile to a fixed transit way (Green Line along University Avenue) and an arterial bus rapid transit (BRT) line (A Line on Snelling Avenue).

Land Use Policy 1.5 comment states, the City should maintain the character of Established Neighborhoods.

The area is a mix of residential and institutional uses including single family homes and apartment buildings of up to three stories as well as churches and schools. A variety of commercial uses are located one block to the west.

Land Use Policy 1.8 states encourage the development of townhouse and smaller multi-family developments, compatible with the character of Established Neighborhoods. The policy comment states, these developments should occur at the intersections of two streets, either arterials or collectors, located on a transit route.

Land Use Policy 1.9 states encourage the development of medium density multi-family housing along Residential Corridors.

Housing chapter of the comprehensive plan

The stretch of Marshall Avenue in which the proposed housing development is located is identified in the housing plan as an opportunity area for potential new housing, Figure H-K.

In light of the need to add more housing units to accommodate an increasing population the housing chapter acknowledges that new market rate housing development and the protection and enhancement of existing neighborhoods are absolutely necessary to maintain and enhance the tax base so vital to the City's ability to fund essential services and infrastructure, page 1.

...greater housing density will be the hallmark of the next 20-30 years. In recognition of the importance of the stability of existing neighborhoods to Saint Paul's future, this density should primarily be geographically focused on transit and commercial corridors such as the Central Corridor, high amenity areas such as Downtown and the Mississippi River Corridor, and a few large scale redevelopment sites such as the Ford assembly plant site, page2.

Saint Paul must build upon its strengths in the metropolitan housing market by encouraging housing that appeals to expanding demographics, uses land efficiently, takes advantage of our amenities, supports transportation choice, and fosters tax base growth, page 7.

Housing Policy 1.1 states increase housing choices across the city to support economically diverse neighborhoods.

Housing Policy 1.3 states revitalize the city by developing land-efficient housing.

Union Park Community Plan

The goal of Union Park Community Plan is to find the balance that preserves desirable assets and neighborhood character while evolving to meet present and future needs. Specific land use policies of the plan include:

LU2. Preserve the well-kept, traditional feel and scale of the neighborhood.

LU2.2 Encourage the continued use and rehabilitation of existing structures, districts, and landscapes to preserve the historic character of residential and commercial districts.

LU2.3 Ensure that new development fits within the character and scale of adjacent neighborhoods.

The housing section of the plan states that it seeks to maintain the District's unique character by promoting intelligent development, encouraging upkeep, preserving character, and adopting a collaborative approach to addressing student housing needs. Specific housing policies of the plan include:

H1. Preserve Union Park's pedestrian-scale neighborhoods, while promoting a range of housing types and affordability to meet the needs of people at different life stages with different housing needs.

H2. Preserve and improve the character and maintenance of Union Park's neighborhoods for the next 10 years and beyond.

H2.2 Encourage rehabilitation of existing housing stock.

H2.2a New residential construction shall be consistent with the character of the surrounding homes, while minimizing impact on the neighborhood.

H3. Recognize and accommodate student-housing needs while respecting the rights and concerns of all community stakeholders (students, families, colleges, landlords, businesses, etc.).

H3.5 Prioritize the development of multi-unit student housing in mixed-use corridors over the expansion of single-family rental units in traditional neighborhoods.

HP1. Integrate historic significance into Union Park's housing, environmental, land use, and economic development decision-making processes, supporting preservation over demolition.



CITY OF SAINT PAUL
Christopher B. Coleman, Mayor

375 Jackson Street, Suite 220
Saint Paul, Minnesota 55101-1806

Telephone: 651-266-8989
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December 14, 2017

Jon Schwartzman
MCR Property Holdings, LLC
10921 Excelsior Blvd., #106
Hopkins, MN 55343

RE: Application for Site Plan Review – SPR #17-206385 – Marshall & Moore Apartments at 1973 – 1977 Marshall Avenue – Notice to extend the time limit for decision under Minnesota Statute 15.99

Dear Mr. Schwartzman,

This letter is to inform you that the City is extending the site plan review period to February 15, 2018.

MN Statute 15.99 (1995) requires the City of Saint Paul to approve or deny zoning applications within 60 days of submission, but allows the City to “extend the time line ... by providing written notice of the extension to the applicant. The notification must state the reasons for the extension and its anticipated length, which may not exceed 60 days unless approved by the applicant.”

On October 18, 2017, you applied for site plan review to develop a new multi-family residential building. Site Plan Review is a function delegated by the St Paul Planning Commission to City staff. However, a Site Plan may be referred to Planning Commission for public hearing if any staff decision on the site plan is likely to be appealed to the Planning Commission. The recommended public hearing date with the Planning Commission for the Site Plan is January 4, 2018.

The City’s present deadline to act on the site plan review application is December 17, 2017. Because this deadline is prior to the January 4, 2018 hearing date, the City elects to extend the deadline for the additional 60 days allowed under Minnesota Statute 15.99. The additional 60-day period takes effect immediately upon the expiration of the initial 60-day period. Therefore, the deadline to make a final decision on your application is February 15, 2018.

If you have any questions regarding this matter, please contact me at 651-266-9086 or tia.anderson@ci.stpaul.mn.us.

Regards,

Tia Anderson
Senior City Planner

cc: File, Zoning Administrator, Planning Manager, Ward 4 Council Office, Union Park District Council

12-14-2017



Tia Anderson
Senior City Planner
City of Saint Paul- Department of Safety and Inspections – Zoning Division
375 Jackson Street, Suite 220
Saint Paul, MN, 55101-1806

**Re: Site Plan Review Report Response Letter, Marshall & Moore Apartments – MCR Property Holdings, LLC
PAI Project No. 51666-17088**

Dear Ms. Anderson:

Please see the responses below in italic, for the Site Plan Review Report prepared by the City of Saint Paul dated November 9, 2017.

2. Zoning

Reviewer: Tia Anderson/651-266-9086 tia.anderson@ci.stpaul.mn.us

Reviewer: Larry Zangs/651-266-9082 larry.zangs@ci.stpaul.mn.us

Comments:

- a) A development moratorium was approved by the City Council on October 18, 2017, on Marshall Avenue between Wheeler and Wilder Streets within the West Marshall Study Area. This project is within the moratorium area, however, a complete Site Plan application was submitted prior to the City Council public hearing on October 18, 2017.

Response: All statements are correct.

- b) The proposed use of the property as Multi-family Housing is permitted at this location in a RM2 medium-density multiple-family residential Zoning District.

Response: This statement is correct.

- c) *Please provide a description of the proposed office operations.* A leasing/maintenance office space serving the multi-family dwelling in which it is located would be considered an accessory use, which is permitted in an RM2 zoning district. However, an office serving multiple residential properties is a general commercial office use, and not permitted in any residential Zoning Districts.

Response: The Owner has stated that the leasing/ maintenance office space will serve the proposed multi-family building in which it is located, to conform to the definition of accessory use.

- d) Tenants of multi-family residential units cannot exceed the definition of a family per Leg. Code Sec. 60.207. Family, which does not allow more than four un-related adults.

Response: The Owner has been informed of this Code Section.

- e) RM2 setback requirements are as follows:

- Minimum front setback is 29' 6" based on the average setback for the block.
- Minimum side setback requirement is 9'.
- Minimum rear setback requirement is 25'.

Response: These setbacks are correct and are being met with this proposed project.

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f) RM2 Density standards are as follows:

- Minimum lot area of 9,000 square feet is required for a multiple-family dwelling with 3 or more dwelling units.
 - One-half the width of a dedicated public alley adjoining the lot shall be considered as part of the lot, for the purpose of applying lot area and density requirements.

Minimum lot area per residential unit is 1,500 square feet. However, the lot area figure may be increased by three hundred (300) square feet for each parking space (up to two parking spaces per unit) within a multiple-family structure or otherwise completely underground.

Response: Please refer to the Site Plan for Unit, Parking, and Lot Area Data.

- The proposed accessory residential garages shall not be included in the lot area bonus calculation nor tandem spaces or other stacked parking.

Response: The accessory residential garages have been eliminated from this proposed project. No tandem spaces or stacked parking spaces are proposed for this project.

g) RM2 Dimensional standards are as follows:

Update the Site Plan with the existing average grade based on the surveyed elevations. Grade is the elevation established for the purpose of regulating the number of stories and the height of buildings. "Grade" shall be the mean level of the finished surface of the ground adjacent to the exterior walls of the buildings.

Response: The Architectural Site Plan has been updated with the existing average grade elevation.

- Maximum building height is 5 stories and 50', as measured from the established grade to the highest point of the roof surface for flat roofs.
 - Where a building is located on sloping terrain (e.g., Moore St elevation), the height may be measured from the average ground level of the grade at the building wall.
 - The existing grade of the property shall not be raised around a new building or foundation in order to comply with the height requirements of this code.
 - The height limitations of this code shall not apply to mechanical service stacks, tanks, ventilation equipment, and similar equipment.

Update the Site Plan to clearly depict the building height and proposed elevations in relation to the proposed structure in order to clarify average grade and height from all elevations.

Response: The Architectural Site Plan and Elevations have been updated with the proposed building height and proposed grade elevations.

h) *Update the Site Plan with the lot coverage percentage.* In residential districts, principal buildings shall not cover more than thirty-five (35) percent of any zoning lot. The lot area may include one-half the width of the dedicated public alley adjoining the lot.

Response: Please refer to the Site Plan for Lot Area Data.

The green roof area exposed on the Moore Street side of the structure, shall be included in the lot coverage calculation.

Response: The green roof area exposed on the Moore Street side of the structure has been eliminated from this proposed project.

Balconies shall be subject to setback and lot coverage requirements. However, per Leg. Code Sec. 63.105., an uncovered porch, deck or patio not exceeding two (2) feet in height is considered landscaping and is not subject to setback or lot coverage requirements.

Response: Balconies are not proposed in the required setbacks and have been included in the Lot Area Data.

- i) Attached vestibules or enclosed entrances may project up to twenty (20) square feet into a required front or rear yard and shall be included as part of the principal structure for lot coverage purposes.

Response: Acknowledged.

3. Off-street Parking

- a) *Update the parking level plan with typical stall dimensions, maneuvering lane width, and number of parking spaces.* Workable parking spaces in the structured parking may be used for calculating the lot area density bonus.

Response: The parking level plans have been updated with the typical stall dimensions, maneuvering lane widths, and number of parking stalls.

- b) *Update the Site Plan analysis with number of residential units and bedrooms, and vehicle and bicycle parking required and provided.* The off-street parking requirement is based on number of residential units and number of bedrooms. 1.5 off-street parking space is required for 2 – 3 bedroom units, and 2 spaces for 4 bedroom units.

- For dwelling units, a minimum of one secure bicycle parking space shall be provided for every 14 dwelling units.

Bicycle parking provided in excess of the minimum required bicycle parking may be substituted for up to 10 percent of minimum off-street vehicle parking requirements. For the purpose of calculating a substitution, 4 spaces in secure bicycle racks are the equivalent of one vehicle parking space.

Response: The Unit, Parking (including bicycle parking), and Lot Area Data have been updated on the Site Plan.

- c) *Confirm location of secure bicycle parking.* Bicycle parking shall be provided in a convenient, safe, and secure location. The site plan included 16 bike racks in the rear of the property. However, at the November 7, 2017, Site Plan Review Committee meeting the project indicated they were considering bicycle parking in the underground parking area. Outdoor bicycle parking shall be visible from the public right-of-way or from inside the building.

Where motor vehicle parking spaces are monitored, covered or weather protected, required bicycle parking spaces shall be provided on the same basis.

Response: The secure bicycle parking is located in the two levels of enclosed parking.

- d) One ADA parking space is required for every 25 standard parking spaces. Parking spaces and passenger loading zones for persons with disabilities shall be designed in accordance with the provisions of the Accessibility Guidelines for Buildings and Facilities of the Americans with Disabilities Act (ADA).

Response: Two ADA and code compliant van accessible spaces with a shared accessible aisle are located in the upper level of enclosed parking.

- e) Accessory parking facilities may designate up to 50 percent of the spaces for compact cars. Compact spaces shall be designated by signs with a minimum of one sign per every four compact spaces. The submitted site plan currently includes 15 compact parking spaces.

Response: Signage complying with this requirement will be installed in the two levels of enclosed parking.

- f) *Update the Site Plan to adhere to Zoning Code requirements for accessory structures specifically as it pertains to the two proposed garages. Accessory buildings, structures or uses shall not be erected in or established in a required yard except a rear yard.*
- On corner lots, accessory structures shall be set back from the street a distance equal to that required of the principal structure.
 - When an accessory building, structure or use is constructed in a rear yard which adjoins a side yard or front yard, the accessory building, structure or use shall be set back from the interior lot line a distance equal to the minimum side yard required of the principal structure.
 - RM2 residential districts, accessory buildings on a zoning lot with residential use shall not exceed fifteen (15) feet in height, measured to the mid-point of the gable; provided, however, that accessory buildings with a flat or shed roof style shall not exceed twelve (12) feet in height.
 - Accessory buildings on a zoning lot may occupy up to thirty-five (35) percent of the rear yard. Rear yards which adjoin alleys may include half the area of the alley to calculate the area of the rear yard which may be occupied by accessory buildings.
 - An accessory building shall not be closer than one foot to an alley right-of-way. Accessory buildings shall be located at least six (6) feet from the principal structure or shall be considered attached for purposes of the zoning code.

Response: The two detached accessory parking structures have been eliminated from this proposed project.

- g) *Update the Site Plan with dimensions and setbacks for the proposed off-street surface parking.* Off-street parking spaces shall not be within a required front or side yard and shall be a minimum of 4 feet from any lot line.

Response: The Site Plan has been updated with dimensions and setbacks for the proposed off-street surface parking.

- h) At the November 7, 2017, Site Plan Review Committee meeting the project indicated that trash and recycling will be wheeled to the curb for pick-up.

Response: Refuse and recycling will be wheeled from the lower level of enclosed parking to the curb for pick-up.

4. Building Design Standards

- a) *Update the site plan with the dimensions of the decorative overhangs.* Overhangs and decorative details may project into a required yard sixteen (16) inches plus two (2) inches for each foot of width of the required side yard.

Response: The Site Plan has been updated with the dimensions of the decorative overhangs.

- b) *Update the site plan with location of the service door to the structured parking in order to meet egress requirements within the parking level.*

Response: The Site Plan has been updated with the service door to provide required egress from the lower level of enclosed parking.

- c) The project shall comply with building design standards per Leg. Code Sec. 63.110:

- A primary entrance of principal structures shall be located within the front third of the structure; be delineated with elements such as roof overhangs, recessed entries, landscaping, or similar design features; and have a direct pedestrian connection to the street.

Response: The primary entrance is located on the Marshall Avenue elevation. The front entry is recessed with an overhang. There is direct pedestrian connection via an exterior concrete stair from the Marshall sidewalk and a walking surface from the easterly driving surface.

- Building materials and architectural treatments used on sides of buildings facing an abutting public street should be similar to those used on principal facades.

Response: Exterior finish materials are proposed as masonry, metal panel, and painted fiber cement panel siding.

- *Provide the percentage of window and door openings on new facades on the SitePlan.*

- For principal buildings, above grade window and door openings shall comprise at least fifteen (15) percent of the total area of exterior walls facing a public street or sidewalk.
- In addition, for new principal residential buildings, above grade window and door openings shall comprise at least ten (10) percent of the total area of all exterior walls.
- Windows shall be clear or translucent.

Response: The Exterior Elevations have; South Elevation=17.21% window and door openings, West Elevation=21.02% window and door openings, North Elevation=19.72% window and door openings, East Elevation=20.68% window and door openings. The six overhead doors are not included as window and door openings.

- The visual impact of rooftop equipment shall be reduced through such means as location, screening, or integration into the roof design. Screening shall be of durable, permanent materials that are compatible with the primary building materials. Exterior mechanical equipment such as ductwork shall not be located on primary building facades.

Response: Acknowledged.

5. Lighting

- a) Exterior lighting shall meet Zoning Code Sec. 63.116. - Exterior lighting.

- All outdoor lighting shall be shielded to reduce glare and shall be so arranged as to reflect lights away from all adjacent residential districts or adjacent residences in such a way as not to exceed three (3) footcandles measured at the residence district boundary.
- All lighting in all districts used for the external illumination of buildings shall be placed and shielded so as not to interfere with the vision of persons on adjacent highways or adjacent property.

Response: Please refer to Sheets E0.2 and E0.2A that demonstrate compliance with these requirements.

6. Landscaping for the Site and Exterior Parking Lot

- a) All required yards and any underdeveloped space shall be landscaped using materials such as trees, shrubs, sod, groundcover plants, or stormwater landscaping.

Response: Appropriate landscaping materials are proposed for this project.

- b) For off-street parking facilities that abut a residential use or zoning district across an alley, one (1) of the following shall be provided and maintained as determined by the zoning administrator as part of site plan review:

- A visual screen 4.5 – 6.5' in height; or
- An ornamental metal fence or other non-screening, durable fence where security concerns make this preferable to a visual screen.

Response: A compliant visual screen consisting of coniferous shrubs or fencing is proposed for this project.

- c) For any parking facility, landscaping shall be provided to buffer the facility from adjacent properties and from the public right-of-way; reduce the visual glare and heat effects of large expanses of pavement; and provide areas for the retention and absorption of stormwater runoff. The standards can be found in Sec. 63.313 and 63.314 of the Zoning Code.

- *Perimeter Landscape* - A landscaped yard at least four (4) feet wide along the public street or sidewalk.
- *Screening Landscape* shall be provided consisting of a masonry wall or decorative fence (not including chain link) between 3' and 4.5' tall along public street or sidewalk supplemented with a landscape buffer.
- *Tree plantings* – At least 1 shade tree shall be planted for every 5 surface parking spaces.

Response: Appropriate perimeter landscaping, screening landscaping, and one shade tree will be provided if the two surface parking stalls are constructed.

7. Parkland Dedication

As a part of this project, the City requires payment of a Parkland Dedication fee (*in lieu of the property owner needing to dedicate a portion of the property as park land*). The fee collected is used by the City for acquiring park land or other park capital improvements in the area.

Staff estimates a dedication fee of \$1689. The fee is calculated based on the number of residential units proposed, number of existing units, and value of the property. Payment is included with building and inspection fees when the building permit is issued.

Response: The Owner has been informed of this fee.

8. Signs

Reviewer: Ashley Skarda/651-266-9013

ashley.skarda@ci.stpaul.mn.us

Comments:

Business signs require a separate review and Sign Permit from the Department of Safety and Inspections. Site plan approval does not constitute approval of signs shown on the site plan. Contact Ashley Skarda of DSI Zoning regarding signs.

Response: The Owner has been informed of this separate review and permitting for signage.

9. Planning

Reviewer: Kady Dadlez/651-266-6619

kady.dadlez@ci.stpaul.mn.us

Comments:

- a) The applicant's property is zoned RM2. An apartment building is a permitted use in the

RM2 zoning district.

Response: These statements are correct.

- b) This stretch of Marshall Avenue is identified as a Residential Corridor in the Land Use Plan, a chapter of the Saint Paul Comprehensive Plan. Residential corridors are described in the plan as segments of street corridors that run through Established Neighborhoods and are predominantly characterized by medium density uses (4 to 30 units per acre). The land use plan adds that some portions of Residential Corridors could support additional housing.

Response: These statements are correct.

- c) The lot area of the property is 15,169 square feet (14,171 square feet plus half of the alley, 998 square feet) or .35 acres. Using the ratio of 30 units per acre, .35 acres would yield 10.5 units. The applicant plans to obtain a density bonus by providing 25 structured parking spaces (25 spaces x 300 square feet), thereby increasing the lot area for density consideration purposes by 7,500 square feet for a total lot area of 22,669 square feet or .52 acres. Using the ratio of 30 units per acre, .52 acres would yield 15.6 units. Sixteen units are proposed. ***Response: Please see the corrections in red, bold, and underlined.***

“The lot area of the property is 15,169 square feet (14,171 square feet plus half of the alley, 998 square feet) or .35 acres. Using the ratio of 30 units per acre, .35 acres would yield 10.5 units. The applicant plans to obtain a density bonus by providing **30** structured parking spaces (**30** spaces x 300 square feet), thereby increasing the lot area for density consideration purposes by **9,000** square feet for a total lot area of **24,169** square feet or **.55** acres. Using the ratio of 30 units per acre, **.55** acres would yield **16.5** units. Sixteen units are proposed.”

- d) Generally speaking the comprehensive plan calls for increased density in residential corridors consistent with the prevailing character and overall density of the area. The Union Park Community Plan, an addendum to the comprehensive plan, calls for finding a balance that preserves desirable assets and neighborhood character while evolving to meet present and future needs. Please refer to the attached document for specific references to land use and housing policies of the comprehensive plan and Union Park Community Plan.

10. District Council

The site is located in the District 13 Community Council. A copy of the site plan was provided to the District Council for comments. A letter from the District Council dated November 6, 2017, is attached for the project's consideration.

Response: Acknowledged.

11. Public Works Records and Mapping

Contact Number: 651-266-6150

Comments:

- a) Provide proof of filing of combination of contiguous lots with Ramsey County.

Response: Proof of filing of combination of contiguous lots will be provided.

- b) Contact Public Works Records and Mapping at 651.266.6150 to determine the address for the building and apartment addresses for the individual units within the building. The building address can be odd numbers within the range of 1973 Marshall to 1979 Marshall.

Response: Public Works Records and Mapping will be contacted to determine the address of the building and apartment addresses for the individual units within the building.

12. Public Works Construction

Reviewer: Scott Brimer/651-266-6225 scott.brimer@ci.stpaul.mn.us

Comments:

Curb at alley entrance must not be placed into alley right of way, but terminate at the property line. Match existing bituminous at alley.

Response: The Site Plan has been updated with this requirement.

13. Public Works Transportation Planning

Reviewer: David Kuebler/651-266-6217

david.kuebler@ci.stpaul.mn.us

Reviewer: Colleen Paavola/651/266-6104

colleen.paavola@ci.stpaul.mn.us

Comments:

- a) Please be advised that a Temporary Pedestrian Access Route (TPAR) and/or a Temporary Traffic Control (TTC) plan may be required as part of the Right-of-Way (ROW) permitting process. Said TTC or TPAR plans must be approved by the City prior to the ROW Permitting office issuing a permit(s).

Response: A TPAR and TTC plan will be provided by the Contractor with the ROW permit application.

- b) Per Minnesota State Statute 326, the final plans submitted must be signed by the appropriate licensed Professional, i.e. PE, LA, PLS, etc., responsible for plan development.

Response: Plans sign by Eric Meyer, P.E. or Larson Engineering

- c) On Plan Sheet S1 please include existing boulevard infrastructure, i.e. signs, lighting, etc. Also show the existing lane striping on Marshall.

Response: An updated survey is enclosed.

- d) On Plan Sheet C1 please include notes regarding the protection of, or impacts to, the existing boulevard infrastructure such as street lighting, signs, etc.

Response: See "Demolition Note" #10 on sheet C1

- e) Please provide turning templates relative to accessing and exiting the proposed three stall garage, the two on-grade stalls and the two stalls in the garage as shown on Plan Sheet C2.

Response: Car path maneuvers are provided for review.

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- f) Please provide detailed dimensions to Plan Sheet C2. The width dimension of the driveway at the ROW line is to be 22 feet unless turning movements indicate something wider is needed.

Response: Dimensions are added to C2.

- g) On Plan Sheet C3.0 please provide a detail of the sidewalk/ramp relationship between ADA compliant ramp that starts at Marshall and parallels the Moore mainline sidewalk.

Response: A ramp to the lower level is not required and had been removed. Lower level access is provided by a sidewalk adjacent to the drive.

- h) On Plan Sheet C3.0 please provide catch basin erosion control devices for the catch basin at the NE quadrant of the Moore/Marshall intersection.

Response: Inlet protection added to the catch basin at the intersection of Moore and Marshall.

- i) On Plan Sheet C5 please update the detail for the driveway apron to a 1206D as can be provided by the City's Sidewalk office. Contact Al Czaia for said detail.

Response: Detail added to sheet Sheet C5.

- j) For the plan sheets completed by Pope Architects, please include the ROW line in the elevation views and verify that what is shown in the elevation views represents what is shown in the Civil plan sheets. For example, the west elevation as provided is different than what is shown on plan sheet C3.0 in that the ADA ramp is not shown and the location of the walk-in door is on the wrong side of the overhead door.

Response: See Pope Architects Sheets.

- k) For the plan sheets completed by Pope Architects, the Garage Plan needs more detail such as grades, drainage information, turning movements showing how vehicles are able to negotiate the area, etc.

Response: See Pope Architects Sheets.

- l) Update the Site Plan with the following notes:

- Change the INSPECTION CONTACT to "Dick Rohland at 651.485.1688".

Response: Contact updated on Sheet C1.

- As part of the ROW permitting process, two weeks before any work begins that impacts the ROW in any way the developer shall provide to the ROW Inspector the name and contact information of the Construction Project Manager or Construction Project Superintendent. If this information is not provided there may be a delay in obtaining permits for the work in the ROW.

Said delays will be the sole responsibility of the developer

- Replace the "ENCROACHMENT" note with the following:
"ENCROACHMENTS: Per Chapter 134 of the Legislative Code, no person shall construct and maintain any projection or encroachment within the public right-of-way.

Construction of the development that necessitates temporary use of the Right-of-Way (ROW) for construction purposes shall be limited to equipment, personnel, devices and appurtenances that are removable following construction.

Encroachment permits will not be granted for devices such as tie backs, rock bolts, H-piles, lagging, timbers, sheet piling, etc. that the owner is seeking to abandon in the ROW.

Section 3201.3 of the Minnesota Building Code defers final authority of encroachments into public rights-of-way/public property to the local authority. City

Legislative Code governs management of the public rights-of-way. Provided such installations are approved by Public Works, footings may be allowed to encroach into City ROW no more than twelve (12) inches at depths below eight (8) feet as provided for in Minnesota Building Code Section 3202.1.

Said encroachments would require an encroachment permit from the City per Chapter 134 of the Legislative Code.

Encroachments into County or State ROW are not allowed unless authorization has been granted from said agency. Marshall Avenue is a County facility.

Response: Notes updated on Sheet C1.

- Change the contact name in the “NO PRIVATE FACILITIES IN THE RIGHT OF WAY” note to Don Bjorkman.

Response: Notes updated on Sheet C1.

- Access to signal controller and lighting cabinets must be maintained at all times. If fencing is required for a job site, a key or other means of access must be provided to the City of St. Paul's Traffic Operations Department. Contact John McNamara, General Foreman Signals and Lighting at 651.266.9780 for more information.

Response: Notes updated on Sheet C1.

- The paragraph that begins “As per the City’s “Standard Specification for Street Openings” needs to be titled “ROADWAY RESTORATION”.

Response: Notes updated on Sheet C1.

- Please include the following notes on the plans:
 - SIGNING: Signs regulating parking and/or traffic on private property shall be installed by the property owner or contractor outside of the public right-of-way (ROW). Removal of signs within the public ROW shall be completed by the City. New signs or the reinstallation of existing signs, as approved by Public Works Traffic Engineering, regulating parking and/or traffic in the public ROW for this development shall be installed by the City at the expense of the development. Contact Chris Gulden of Public Works 651-266-9778 two weeks in advance of needed sign work.

Response: Notes updated on Sheet C1.

- METERS: The City shall be contacted if development work affects the parking meter system. Said system includes meters, space numbers, pay stations and all appurtenances needed for the system to be operational. Removal of meter system elements within the public ROW shall be completed by the City and charged to the developer. The installation of new meters or the reinstallation of removed meters shall also be completed by the City and charged back to the developer. The developer/contractor shall contact Chris Gulden at 651.266.9778 two weeks in advance of needed meter work. If two weeks’ notice is not provided there may be a delay in obtaining permits for the work in the ROW. Said delays will be the sole responsibility of the developer.

Response: Notes updated on Sheet C1.

m) CITY OF ST. PAUL PERMIT REQUIREMENTS:

- ORDERING OBSTRUCTION AND EXCAVATION PERMITS: Contact Public Works Right of Way Service Desk at (651) 266-6151. It is strongly recommended that contractors call for cost estimates prior to bidding to obtain accurate cost estimates.

Response: Notes updated on Sheet C1.

- **OBSTRUCTION PERMITS:** The contractor must obtain an Obstruction Permit if construction (including silt fences) will block City streets, sidewalks or alleys, or if driving over curbs.
Response: Notes updated on Sheet C1.
- **EXCAVATION PERMITS:** All digging in the public right of way requires an Excavation Permit. If the proposed building is close to the right of way, and excavating into the right of way is needed to facilitate construction, contact the utility inspector.
Response: Notes updated on Sheet C1.
- **FAILURE TO SECURE PERMITS:** Failure to secure Obstruction Permits or Excavation Permits will result in a double-permit fee and other fees required under City of St. Paul Legislative Codes.
Response: Notes updated on Sheet C1.

14. Public Works Sidewalks

Reviewer: Al Czaia/651-266-6108 al.czaia@ci.stpaul.mn.us

Comments:

- a) Contractor is responsible for damage to the mainline sidewalk, curb, drive access and boulevard landscaping cause during the construction. Contractor advised to document pre-existing condition of the right of way prior to commencement of the construction.
Response: Notes updated on Sheet C1.
- b) Sidewalk grades must be carried across driveways.
Response: Notes updated on Sheet C1.
- c) Update the Site Plan with the following notes:
 - **CONSTRUCTION IN RIGHT OF WAY:** All work on curbs, driveways, and sidewalks within the public right of way must be done to City Standards and Specifications by a contractor licensed to work in the City right-of-way under a permit from Public Works Sidewalk Section (651-266- 6108). Sidewalk grades must be carried across driveways.
Response: Notes updated on Sheet C1.
 - **RIGHT OF WAY RESTORATION:** Restoration of asphalt and concrete pavements are performed by the Public Works Street Maintenance Division. The contractor is responsible for payment to the City for the cost of these restorations. The contractor shall contact Public Works Street Maintenance to set up a work order prior to beginning any removals in the street at 651- 266-9700. Procedures and unit costs are found in Street Maintenance's "General Requirements - All Restorations" and are available at the permit office.
Response: Notes updated on Sheet C1.

15. Ramsey County

Reviewer: Erin Laberee/651-266-7105 erin.laberee@co.ramsey.mn.us

Comments:

A copy of the Site Plan was provided to Ramsey County for review. Ramsey County has no comments on the plan as submitted.

16. Metro Transit

Reviewer: Berry Farrington/612-349-7378 berry.farrington@metrotransit.org

Comments:

- a) The plans show the developer is responsible for constructing a replacement boarding pad for the bus stop. At minimum it must be 5 feet wide by 8 feet deep and meet the standards identified by the Americans with Disabilities Act of 1990.

Response: See concrete pad place at the northeast corner of the Marshall/Moore Intersection.

- b) The retaining wall is adjacent to a bus stop and the first level of the wall may be used as seating. Metro Transit suggests the wall design include surface for seating which will help protect the landscaping from damage.

Response: The retaining wall is adjacent to the sidewalk and will be at 19" tall to accommodate seating.

- c) Please coordinate any needed short term lane closures with Metro Transit Street Operations, Greg Tuveson (612-349-5407 / [greg.tuveson@metrotransit.org](mailto:ggreg.tuveson@metrotransit.org))

Response: Notes updated on Sheet C1.

17. Public Works Sewers

Reviewer: Anca Sima/651-266-6237

anca.sima@ci.stpaul.mn.us

Comments:

- a) Add a note on the demolition plan: removal permit for A11882 & 12719: cut off at the property line.

Response: Notes updated on Sheet C1.

- b) Provide the detailed drainage map and the HydroCad calculations (some roof ponding)

Response: See attached drainage map and HydroCAD calculations.

- c) The EOF should be at least 1 ft. lower than FFE. Show the EOF on the plan.

Response: Underground Stormwater storage lowered to maintain at least 1 ft of separation.

- d) If the existing manhole for the proposed sanitary service connection is a brick construction, reconstruction of the manhole access may be required, please add the attached detail (2322e) for manhole reconstruction.

Response: The '2322F Manhole Reconstruct' was added to the plans, from the city website. If "e" is required please provide the necessary detail, as it is not on the website.

- e) The proposed man hole should be further than 50ft away from the existing one, if not, connect the proposed connection into an existing one.

Response: The Storm sewer line was changed to an 8" PVC in hope of a wye/saddle connection to the existing 21" RCP.

- f) Add the city's standard plate with the connection at the main.

Response: See connection detail added to Sheet C5

- g) If the storm pipe is less of 5" of building foundation, than is under the plumbing jurisdiction. It should be a steel pipe and you have to provide 5 pound air test.

Response: No storm pipe is within 5" of building

- h) Show that all the storm water will be captured in the trench for 100 years event.

Response: A capacity calculation for the trench drain is included in the stormwater calculations. At 1" depth the drain has a capacity of 4.6 cfs and the runoff from the entire site in a 100-year event (before the stormwater system) is 1.52 cfs.

- i) Sanitary and/or storm sewer service passing within 10 feet of the building are governed by the MN Plumbing Code. Specification for pipe material selection and notes for required air test of the piping, compliant with the MN State Plumbing Code, must be shown on the plan.
Response: Notes are provided on the plan for the storm sewer testing requirements.

j) Update the Site Plan with the following notes:

- SEWER REPAIR PERMIT: Plumbing Contractor to obtain "Repair Permits" from Public Works for proposed modification to the existing storm sewer connections. Call St Paul PW permit desk (651-266-6234) for information on obtaining this permit.
Response: See permitting notes added to Sheet C4.
- SEWER REMOVAL/ABANDONMENT PERMIT: Plumbing Contractor to obtain "Removal Permits" from Public Works to cut off existing sewer connections services to the property. Call St Paul PW permit desk (651-266-6234) for information on obtaining this permit.
Response: See permitting notes added to Sheet C4.
- SEWER CONNECTION PERMIT: License house drain contractor to obtain (Sewer Connection Permit) to construct new sanitary and storm connection in street from main to the property. Call St Paul PW permit desk (651-266-6234) for information on obtaining this permit.
Response: See permitting notes added to Sheet C4.

18. Water Quality/Erosion Control

Reviewer: Wes Saunders-Pearce/651-266-9112 wes.saunders-pearce@ci.stpaul.mn.us

Comments:

- a) Erosion control plan must show silt fence / perimeter controls within the private property not the public boulevard.
Response: Silt Fence has been moved to within the property lines.
- b) Erosion control plan must add inlet protection to catch basins on Moore and Marshall.
Response: Inlet protection was added to 4 catch basins at the intersection of Moore/Marshall
- c) Provide more details on green roof including specific landscaping proposed and design cross section. Consider provisions for signage or fencing to prevent inadvertent impact by occupants.
Response: The green roof has been changed to surface pavement for access to main floor parking.
- d) There must be 1 foot of separation from the 100-yr high water level and low floor.
Response: Underground Stormwater storage lowered to maintain at least 1 ft of separation.

19. Water Utility

Reviewer: Jeff Murphy/651-266-6813 jeffrey.murphy@ci.stpaul.mn.us

Comments:

- a) Show existing water services on Plan Sheet C1 or C4 to be cut off at the water main.
- 1977 Marshall Ave – 1" CP – 30 S N Marshall / 28 E E Moore
 - 1973 Marshall Ave – 1" CP – 80 E E Moore
- Response: Water services are shown on the plan.**
- b) The following work shall be performed by SPRWS on an actual cost basis. An estimate will be provided and payment in the amount of the estimate must be received before the work can be

scheduled. Work of this type is currently being scheduled 4 to 6 weeks after payment and required signatures have been received:

- Pipework within right of way for *4" ductile iron domestic service*.
- Pipework within right of way for *6" ductile iron fire service*.
- Cut off of existing unused water services at the main.
- Inspection of water facility work performed by owner's contractor.

Response: acknowledged, no plan changes needed.

- c) Plumbing permit applications to be made with SPRWS at 1900 Rice Street, Saint Paul, MN.

Response: acknowledged, no plan changes needed.

- d) Before construction of a new water service can be scheduled, SPRWS must receive a Water Service Contract signed by the owner and all required payments.

Response: acknowledged, no plan changes needed.

- e) Provide completed project data sheets to determine meter sizing.

Response: acknowledged, no plan changes needed.

- f) Furnish one set of interior fire suppression mechanical plans for review and approval by SPRWS plumbing inspection unit.

Response: acknowledged. Final plans will be provided when complete.

- g) Furnish one set of revised site plans for review. Following approval by SPRWS, furnish one set of approved plans.

Response: acknowledged. Final plans will be provided when mechanical design is complete.

- h) Provide the following on PLAN SHEET C4 under NOTES:

- Water services to be installed according to SPRWS "Standards for the Installation of Water Mains.
- A four-sided trench box is required on all excavations deeper than 5 feet where underground work or inspection is to be performed by SPRWS. Ladders are required and must extend 3 feet above the surface of the trench. Sidewalks, pavements, ducts and appurtenant structures shall not be undermined unless a support system or another method of protection is provided. Trenches in excess of 20 feet in depth must be signed off by a registered professional engineer. Excavated material must be kept a minimum of 2 feet from the edge of the trench.
- Pipe material for 8" Ductile Iron Pipe must be Class 52, Pipe material for 6" and 4" Ductile Iron Pipe must be Class 53. The exterior of ductile iron pipe shall be coated with a layer of arc-sprayed zinc per ISO 8179. The interior cement mortar lining shall be applied without asphalt seal coat.
- Pipe must be wrapped in V-Bio Polywrap encasement.
- Maintain 3 feet vertical separation between water and sewer pipes or a 12 inch separation with 4 inch high density insulation per SPRWS Standard Plate D-10 for typical water main offsets.
- Refer to SPRWS "Standards for the Installation of Water Mains" Standard Plate D-11 for restrained pipe requirement.
- All water service valve boxes within construction area must be exposed and brought to grade upon completion of construction.
- All pipe work inside of property to be performed by a plumber licensed by the State of

Minnesota and Certified by the City of Saint Paul. SPRWS requires separate outside and inside plumbing permits for each new water service.

- All unused existing water services to be cut off by SPRWS. Excavation and restoration by owner's contractor. New water services will not be turned on until required cutoffs have been performed.
- Water facility pipework within right of way to be installed by SPRWS. Excavation and restoration by owner's contractor.
- The contractor providing excavation is responsible for obtaining all excavation and obstruction permits required by any governing authority.

Response: See updated "Utility Notes" on Sheet C4.

20. Fire

Reviewer: Ann McLoughlin-651-266-9140 ann.mcloughlin@ci.stpaul.mn.us

Comments:

- a) Visibility of the Fire Department connection may be located on the N Moore St side of the building as close to Marshall Ave as possible in a visible, accessible location at all times without obstruction by fences, bushes, trees, walls, or other objects for a minimum of 3 feet to the front and each side. This requirement is applicable for the duration of the construction time as well.

Response: The Architectural Site Plan has been updated with these requirements

- b) A metal sign or collar with raised letters at least 1 inch in size must be mounted on all Fire Department connections serving fire sprinklers, standpipes, or fire pump connections. Such signs shall read AUTOMATIC SPRINKLERS or STANDPIPES or TEST CONNECTION or a combination thereof as applicable.

Response: The Architectural Site Plan has been updated with this requirement.

- c) Automatic Fire Sprinkler System Required. Plans and permits required by licensed contractor. Flow test is required to determine adequacy of water service for proposed fire suppression system. The fire prevention section of the Dept. of Safety and Inspections reviews sprinkler plans, issues permits and does the necessary inspections of the installation. Contact Jeff Hemenway 651-266- 8952 with questions on obtaining this permit and the procedure for arranging a time to perform this test.

Response: Acknowledged

- d) Update the Site Plan with the following notes:

Contractor to maintain access to the fire department connection for fire department personnel at all times during the construction period.

Response: The Architectural Site Plan has been updated with this note.

21. City Forestry

Reviewer: Zach Jorgensen/651-632-2437 zach.jorgensen@ci.stpaul.mn.us

Comments:

- a) Street tree removals:

- Review utility placement and impacts to street trees on Marshall. If excavation for underground utility lines will be within 8ft of the trunk of a tree then removal and replacement will be required. Demo and landscape plans are to be updated accordingly. Contact me with questions.

Response: Utilities moved to minimize demolition of existing trees.

- b) Update the landscape plan:
- Identify the tree types to be planted. Street trees are to be swamp white oak or hackberry.
 - Include a plant schedule.
 - Trees planted on the boulevard are to be 2" caliper and have a minimum one year warranty.

Response: See updated Sheet C2.

- c) Ensure tree protection fencing is maintained throughout the construction process.

Response: See updated Demolition Note" #8 on sheet C1.

- d) Existing street trees are to be protected at all times. Trees damaged or removed during construction shall be restored or replaced to the satisfaction of, and at no cost to, the City as determined by the Forestry manager.

Response: See updated Demolition Note" #8 on sheet C1.

- e) Boulevard restoration is to include the following:

- Where driveways, sidewalks or other surface paving are removed all concrete, asphalt and base materials shall be removed.
- Boulevard soils are to be protected during construction. Soil compaction due to construction activities shall be mitigated and soils loosened prior to final grading.
- Boulevards shall be restored with a minimum of 4" of topsoil.

Response: See updated "Landscaping Notes" on sheet C2

- f) Concrete washouts shall not be located within the drip line of a tree.

Response: See "Erosion Control Notes" #21 on Sheet C3.

- g) Update the Site Plan with the following notes (notes are to be included on the demo and landscape plan sheets):

- The removal, pruning, and/or planting of trees on the public boulevard requires an approved permit from the City Forester (651-632-2437). Any work must be completed by a licensed tree contractor.

Response: See updated "Demolition Notes" #6 on Sheet C1.

- Construction supplies, materials, spoils, equipment, and vehicles shall not be stored or operated within the drip line of any public street tree or on turf boulevards without prior written approval from the City Forester. If the boulevard must be used for construction activities, site access routes, material storage or other related activities, protective measures approved by the City Forester shall be taken to reduce soil compaction and protect tree(s) from damage.

Response: See updated "Demolition Notes" #7 on Sheet C1.

- Street trees shall be protected by establishing a tree protection zone using 4' tall fencing installed at the drip line of the tree. Tree protection fencing shall be installed prior to the start of any site work and maintained for the duration of the project. Proposed work within, or changes to the location of tree protection fencing shall be reviewed by the City Forester prior to alteration.

Response: See updated "Tree Protection Detail" on Sheet C5 and updated "Demolition Notes" #8.

- The location of tree protection fences shall be shown on the plan.

Response: See updated Demolition Plan on Sheet C1.

- A tree protection fence detail shall be included in the plan set (see attached).

Response: See updated “Tree Protection Detail” on Sheet C5

- Contractor shall contact the City Forester (651-632-2437), prior to demolition or other land disturbance associated with site construction, to verify tree protection measures.

Response: See updated “Demolition Notes” #9.

- Tree planting details shall include the following:
 - Expose root flare and set at grade.
 - Remove burlap and ropes from top 1/3rd of root ball, cut wire basket down to second horizontal wire from the bottom, and dispose of off-site.
 - Contractor is responsible to maintain trees in a plumb position throughout the maintenance period.

Response: See updated planting note on Sheet C5.

22. Parks and Recreation

Reviewer: Paul Sawyer/651-266-6417 paul.sawyer@ci.stpaul.mn.us

Comments:

No concerns

23. Plumbing

Reviewer: Rick Jacobs/651-266-9051 rick.jacobs@ci.stpaul.mn.us

Comments:

- a) Contact Saint Paul Regional Water Services (SPRWS) for questions, permits, fees, inspections, specifications, plans, or information that may be required for the water service and/or the water meter.

Response: Acknowledged

- b) No utilities must run under any accessory structures.

Response: Accessory buildings have been removed from the project

- c) The accessory buildings used to store or house vehicles must not have drains and the floors must be pitched toward the outside via the opening for overhead garage door.

Response: Accessory buildings have been removed from the project

- d) Sanitary and/or storm sewer service passing within 10 feet of the building are governed by the MN Plumbing Code. Specification for pipe material selection and notes for required air test of the piping, compliant with MN State Plumbing Code 4714 Section 1109.0, must be shown on the plan. This system must be reviewed and approved by Rick Jacobs, Senior Plumbing Inspector (651-266- 9051) at the time of the Plumbing Plan review to ensure that it meets Plumbing Code standards.

Response: Acknowledged

- e) Contact the City of Saint Paul Department of Public Works Sewer Division for questions, permits, fees, inspections, specifications, plans, or information that may be required for sewer and storm piping work performed outside the building, including “retention systems”.

Response: Acknowledged

- f) All primary roof drains shall be piped internally to below grade, exit the building, and be connected to the storm sewer. MPC 4714.1101.1.

Response: Acknowledged

- g) Secondary Roof Drainage shall drain to an approved place of disposal in the form of Secondary Roof Drains installed per MPC 4714.1101 & 1102, and Minnesota State Building Code 1503.4 1-5. Secondary roof drainage must discharge onto permeable soils and cannot drain onto the sidewalk. MPC 4714.1101.1. An approved place of disposal that can be approved by the Authority Having Jurisdiction for secondary roof drainage is in the form of secondary roof drains piped internally, down to within 18 inches of grade, through the outside wall, onto a splash block installed per MPC 1101.5.3, and laid over permeable soils of an adequate amount where saturation of the soil will not occur.

Response: Acknowledged

- h) The “green roof” design will require approval of an Alternate Code Compliance under MPC 4714 301.2 as it is not a design approved by the plumbing code. Provide a proposed design and completed DSI Alternate Code Compliance form to the DSI Plumbing Section for approval prior to site plan approval. All discharge to the “green roof” area must be included in the total drainage calculations of the green roof.

Response: The “green roof” has been eliminated from this proposed project.

24. Building Code Requirements

Reviewer: James Williamette/651-266-9077 james.williamette@ci.stpaul.mn.us

Comments:

- a) This proposal will require a demolition permit to remove the existing structures and a building (grading) permit to proceed with any grading activity.

Response: Acknowledged

- b) This proposal will require a building permit to proceed. The building permit is issued only after all necessary city staff have approved and signed off on the proposed design. In addition to the building permit, separate permits are required for any plumbing, electrical and mechanical work, elevator installation and any fire sprinkler modifications. These permits must be obtained and the work performed by city licensed contractors in each of the respective trades.

Response: Acknowledged

- c) One PDF and two sets of complete construction documents stamped by public works must be submitted with the building permit application to the DSI Main Office/ Permit Desk.

- The construction documents shall include architectural, structural, mechanical, electrical and plumbing plans signed and stamped by design professionals registered in the State of Minnesota. The architect shall provide a complete code analysis and a color-coded exit plan showing all fire rated walls and shafts and include exit access and travel distances. The plans shall have the energy code noted on the plans along with the compliance path chosen. The submittal should include compliance documents detailing how the energy code requirements are met.
- Plans must be dimensioned, drawn to scale and sufficiently detailed to denote the scope of work to be performed and the method of construction.
- Mechanical ventilation plans will need to be prepared by a mechanical engineer, registered with the State of Minnesota. In some cases, a “Master in the Trade” may prepare plans. The ventilation contractor should contact our senior warm air inspector Gary Reinsberg (651-266- 9064) or by e-mail at Gary.reinsberg@ci.stpaul.mn.us The

Energy code and path must be noted on these plans also.

- The plumbing and electrical contractors for this project should contact our office if they have questions about whether engineered plans need to be submitted with their permit request. The senior plumbing inspector is Rick Jacobs at 651-266-9051 Rick.jacobs@ci.stpaul.mn.us and the senior electrical inspector is Dan Moynihan at 651-266-9036 Dan.monihan@ci.stpaul.mn.us

Response: Acknowledged

Note: **The building permit will not be issued until all signoffs are received** from Zoning, HPC, HVAC, Public Works, or other departments that are assigned to the project.

Response: Acknowledged.

25. Service Availability Charge (SAC)

The proposed project will need a SAC determination before a building permit can be issued. You must submit a copy of the plans to the Metropolitan Council Environmental Services (MCES) for a SAC determination. Please see their website at <https://metro council.org> for additional information. If MCES determines that a SAC fee is due, the City will collect that payment with the building permit fee.

SAC Questions and Determination Review Submittal Information email:

SACprogram@metc.state.mn.us or call 651-602-1770 to speak to a SAC representative.

Response: The City of Saint will need to report the demolition of the two existing properties to the Metropolitan Council. Pope Architects will submit for a new determination for the proposed multi-family building.

Sincerely,

POPE ARCHITECTS, INC.

Paul Hanlon
Project Architect

cc: File, Jon Schwartzman, Eric Meyers



CITY OF SAINT PAUL
Christopher B. Coleman, Mayor

375 Jackson Street, Suite 220
St Paul, Minnesota 55101-1806

Telephone: 651-266-8989
Facsimile: 651-266-9124
Web: www.stpaul.gov/dsi

December 20, 2017

Jon Schwartzman
MCR Property Holdings, LLC
10921 Excelsior Blvd., #106
Hopkins, MN 55343

Paul Hanlon
Pope Architects
1295 Bandana Blvd., Ste 200
St Paul, MN 55108

Eric Meyers
Larson Engineering
3524 Labore Rd
White Bear Lake, MN 55110

RE: **Updated Site Plan 17-206385** – Marshall & Moore Apartments at 1973 – 1977 Marshall Ave – Site Plans with revisions through 12/14/2017.

Mr. Schwartzman, Mr. Hanlon, and Mr. Meyers,

A majority of the Site Plan Review Committee has had an opportunity to review the updated plan set and provide feedback. Below is a summary of the outstanding requirements or issues identified. Any subsequent feedback will be forwarded in a revised memo.

General Comments

1. Prior to Final Site Plan Approval, the following shall be required:
 - Per Minnesota State Statute 326, the final plans submitted shall be signed by the appropriate licensed Professional, i.e. PE, LA, RLS, etc., responsible for plan development.
 - Final plans shall not be marked “preliminary” or “not for construction.”
 - Provide proof of filing of combination of contiguous lots with Ramsey County.
 - Contact Public Works Records and Mapping at 651.266.6150 to determine the address for the building and apartment addresses for the individual units within the building. The building address can be odd numbers within the range of 1973 Marshall to 1979 Marshall.

Zoning

Tia Anderson/651-266-9086 tia.anderson@ci.stpaul.mn.us

Larry Zangs/651-266-9082 larry.zangs@ci.stpaul.mn.us

2. It would be helpful to letter or number the Architectural Plan Sheets for ease of reference.
3. Update the Data on the Site Plan sheet as follows:
 - Under “parking required” 3 – 3BR units is listed instead of 6 – 3BR units
 - Indicate amount of bicycle parking provided (as referenced in the 12/14/17 Response letter)
4. Clearly label the Garage Plan and Ground Floor Plan sheets with typical parking dimensions, maneuvering lanes, and location of bicycle parking.

- On the 12/14/17 Garage Plan there appears to be extra lines on the North side of the drawing, and a 9' parking stall dimension on the South side that is located incorrectly.
 - Update the maneuvering lane width for the stall near the elevator lobby. The minimum width is 20'. A 22' dimension is shown for the adjoining stall.
5. Update the West and South elevations to identify the building height to the top of the roof deck. For purposes of presenting to the Planning Commission, it may be useful to depict the height from both the existing average grade as well as the proposed grade.
 6. Update C2 with key dimensions, including: setbacks, driveway access widths, ground level driveway width.
 7. Paving and landscaping shall be consistent between the Architectural Site Plan, Site Plan, and C2 sheets.
 - Currently the rear yard snow storage area/slab and paving near the front sidewalk are inconsistent between the plan sheets.
 - The Architectural Site Plan includes a 4' 6" wood screening fence; C2 includes a 6' composite screening fence. Visual screen locations shall conform with front yard setback lines in residential districts. Please add a detail for the proposed fence.
 - The Architectural Site Plan refers to a retaining wall on the northeast property line near the snow storage with a note to "see Civils." The wall is not referred to on the Civil plan sheets. Please add a detail of the wall.
 8. The dimensions of the proposed decorative overhang at the building entrance may need to be modified. Overhangs and decorative details may project into a required yard sixteen (16) inches plus two (2) inches for each foot of width of the required side yard, which for this property is 9', allowing for up to a 34" overhang.
 9. Confirm whether the proposed surface parking shall remain prior to Final Site Plan Approval. Based on the current unit size and mix, 29 off-street parking spaces are required. A decision on the surface parking may be impacted by Accessibility Code requirements. Note that the stormwater drainage calculations may need to be updated to reflect the final decision.

Public Works Transportation Planning

David Kuebler/651-266-6217 david.kuebler@ci.stpaul.mn.us

10. Please provide turning movement templates as requested in the 11/09/2017 Site Plan Review Report. Turning templates should be provided for maneuvering within all parking areas.
11. Please see the attached documents for redlines of the revised plans for the Marshall & Moore Apartments as well as applicable redlined pages of the 11/9/17 Site Plan Review Report.

Public Works Sewers

Anca Sima/651-266-6237 anca.sima@ci.stpaul.mn.us

12. The 12/14/17 site plan is still under review. Any additional comments or questions will be forthcoming.

Plumbing

Rick Jacobs/651-266-9051 rick.jacobs@ci.stpaul.mn.us

13. Please add the following notes to the Site Plan Notes:
 - Contact Saint Paul Regional Water Services (SPRWS) for questions, permits, fees, inspections, specifications, plans, or information that may be required for the water service and/or the water meter.

- Sanitary and/or storm sewer service passing within 10 feet of the building are governed by the MN Plumbing Code. Specification for pipe material selection and notes for required air test of the piping, compliant with MN State Plumbing Code 4714 Section 1109.0, must be shown on the plan. This system must be reviewed and approved by Rick Jacobs, Senior Plumbing Inspector (651-266-9051) at the time of the Plumbing Plan review to ensure that it meets Plumbing Code standards.
- Contact the City of Saint Paul Department of Public Works Sewer Division for questions, permits, fees, inspections, specifications, plans, or information that may be required for sewer and storm piping work performed outside the building, including “retention systems”.
- All primary roof drains shall be piped internally to below grade, exit the building, and be connected to the storm sewer. MPC 4714.1101.1.
- Secondary Roof Drainage shall drain to an approved place of disposal in the form of Secondary Roof Drains installed per MPC 4714.1101 & 1102, and Minnesota State Building Code 1503.4 1-5. Secondary roof drainage must discharge onto permeable soils and cannot drain onto the sidewalk. MPC 4714.1101.1. An approved place of disposal that can be approved by the Authority Having Jurisdiction for secondary roof drainage is in the form of secondary roof drains piped internally, down to within 18 inches of grade, through the outside wall, onto a splash block installed per MPC 1101.5.3, and laid over permeable soils of an adequate amount where saturation of the soil will not occur.

Water Utility

Jeff Murphy/ 651-266-6276 jeffrey.murphy@ci.stpaul.mn.us

14. Per the 11/09/2017 SPR Report, show existing water services on Plan Sheet C1 or C4 to be cut off at the water main.
 - 1977 Marshall Ave – 1" CP – 30 S N Marshall / 28 E E Moore
 - 1973 Marshall Ave – 1" CP – 80 E E Moore

Forestry

Zach Jorgensen/651-632-2437 zach.jorgensen@ci.stpaul.mn.us

15. The existing street trees on Marshall Avenue are not consistently shown in the updated plan set (architectural and civils). Please include the trees in the site plan and indicate whether they will be removed/replaced or protected during construction.

Metro Transit

Berry Farrington/612-349-7378 berry.farrington@metrotransit.org

16. Consider adding a cap to the retaining wall adjacent to the sidewalk near the corner at Marshall and Moore. A cap of up to 24" deep would help protect the private landscaping from damage that could occur from people sitting on the wall.

If you have questions, please contact me at 651-266-9086 or tia.anderson@ci.stpaul.mn.us.



Tia Anderson
Senior City Planner

cc: File

December 28, 2017

Tia Anderson
Senior City Planner
City of Saint Paul – Department of Safety and Inspections – Zoning Division
375 Jackson Street, Suite 220
Saint Paul, MN 55101-1806

RE: Site Plan Review Report Response Letter, Marshall & Moore Apartments at 1975 Marshall Avenue. MCR Property Holdings, LLC. Site Plans with revisions through 12/14/2017.

Ms. Anderson,

Please see the responses below in bold italics for the Site Plan Review Report prepared by the City of Saint Paul dated December 20, 2017

A majority of the Site Plan Review Committee has had an opportunity to review the updated plan set and provide feedback. Below is a summary of the outstanding requirements or issues identified. Any subsequent feedback will be forwarded in a revised memo.

General Comments

1. Prior to Final Site Plan Approval, the following shall be required:
 - Per Minnesota State Statute 326, the final plans submitted shall be signed by the appropriate licensed Professional, i.e. PE, LA, RLS, etc., responsible for plan development.
Response: The final plans have been signed by the appropriate licensed professional
 - Final plans shall not be marked “preliminary” or “not for construction.”
Response: Final plans are not marked “Preliminary” or “Not For Construction”.
 - Provide proof of filing of combination of contiguous lots with Ramsey County.
Response: Proof of filing of combination of contiguous lots will be provided.

- Contact Public Works Records and Mapping at 651.266.6150 to determine the address for the building and apartment addresses for the individual units within the building. The building address can be odd numbers within the range of 1973 Marshall to 1979 Marshall.

Response: Public Works Records and Mapping has been contacted. The address have been confirmed as 1975 Marshall Avenue with the sub-addresses for the Apartments as 101, 201, 202.....

Zoning

Tia Anderson/651-266-9086 tia.anderson@ci.stpaul.mn.us

Larry Zangs/651-266-9082 larry.zangs@ci.stpaul.mn.us

2. It would be helpful to letter or number the Architectural Plan Sheets for ease of reference.

Response: Architectural Plan Sheets have been numbered.

3. Update the Data on the Site Plan sheet as follows:

- Under “parking required” 3 – 3BR units is listed instead of 6 – 3BR units

Response: This information has been amended to 6-3BR units.

- Indicate amount of bicycle parking provided (as referenced in the 12/14/17 Response letter)

Response: The amount of bicycle parking has been added to the Site Plan Data.

4. Clearly label the Garage Plan and Ground Floor Plan sheets with typical parking dimensions, maneuvering lanes, and location of bicycle parking.

Response: Typical parking dimensions have been added and the location of bicycle parking has been noted. Seven pages have been added to demonstrate turning movements for the two levels of parking.

- On the 12/14/17 Garage Plan there appears to be extra lines on the North side of the drawing, and a 9’ parking stall dimension on the South side that is located incorrectly.

Response: These items have been amended.

- Update the maneuvering lane width for the stall near the elevator lobby. The minimum width is 20’. A 22’ dimension is shown for the adjoining stall.

Response: This item has been amended.

5. Update the West and South elevations to identify the building height to the top of the roof deck. For purposes of presenting to the Planning Commission, it may be useful to depict the height from both the existing average grade as well as the proposed grade.

Response: These items have been added to the West and South Elevations.

6. Update C2 with key dimensions, including: setbacks, driveway access widths, ground level driveway width.

Response: This information has been added to Sheet C2.

7. Paving and landscaping shall be consistent between the Architectural Site Plan, Site Plan, and C2 sheets.

Response: Information on the Design Site Plan, Architectural Site Plan, and the C2 Sheet has been coordinated.

- Currently the rear yard snow storage area/slab and paving near the front sidewalk are inconsistent between the plan sheets.

Response: This area has been coordinated.

- The Architectural Site Plan includes a 4' 6" wood screening fence; C2 includes a 6' composite screening fence. Visual screen locations shall conform with front yard setback lines in residential districts. Please add a detail for the proposed fence.

Response: A 6' high wood fence is proposed. A detail has been added.

- The Architectural Site Plan refers to a retaining wall on the northeast property line near the snow storage with a note to "see Civils." The wall is not referred to on the Civil plan sheets. Please add a detail of the wall.

Response: This retaining wall has been added to the Civil plan sheets and a detail has been added to the Civil plan sheets.

8. The dimensions of the proposed decorative overhang at the building entrance may need to be modified. Overhangs and decorative details may project into a required yard sixteen (16) inches plus two (2) inches for each foot of width of the required side yard, which for this property is 9', allowing for up to a 34" overhang.

Response: The overhang and the associated dimensions have been amended.

9. Confirm whether the proposed surface parking shall remain prior to Final Site Plan Approval. Based on the current unit size and mix, 29 off-street parking spaces are required. A decision on the surface parking may be impacted by Accessibility Code requirements. Note that the stormwater drainage calculations may need to be updated to reflect the final decision.

Response: The off-street surface parking has been eliminated from the proposed project.

Public Works Transportation Planning

David Kuebler/651-266-6217 david.kuebler@ci.stpaul.mn.us

10. Please provide turning movement templates as requested in the 11/09/2017 Site Plan Review Report. Turning templates should be provided for maneuvering within all parking areas.

Response: Seven pages have been added to demonstrate turning movements for the two levels of parking.

11. Please see the attached documents for redlines of the revised plans for the Marshall & Moore Apartments as well as applicable redlined pages of the 11/9/17 Site Plan Review Report.

Response: These redlines have been addressed.

Public Works Sewers

Anca Sima/651-266-6237 anca.sima@ci.stpaul.mn.us

12. The 12/14/17 site plan is still under review. Any additional comments or questions will be forthcoming.

Response: Acknowledged

Plumbing

Rick Jacobs/651-266-9051 rick.jacobs@ci.stpaul.mn.us

13. Please add the following notes to the Site Plan Notes:

- Contact Saint Paul Regional Water Services (SPRWS) for questions, permits, fees, inspections, specifications, plans, or information that may be required for the water service and/or the water meter.
- Sanitary and/or storm sewer service passing within 10 feet of the building are governed by the MN Plumbing Code. Specification for pipe material selection and notes for required air test of the piping, compliant with MN State Plumbing Code 4714 Section 1109.0, must be shown on the plan. This system must be reviewed and approved by Rick Jacobs, Senior Plumbing Inspector (651-266-9051) at the time of the Plumbing Plan review to ensure that it meets Plumbing Code standards.
- Contact the City of Saint Paul Department of Public Works Sewer Division for questions, permits, fees, inspections, specifications, plans, or information that may be required for sewer and storm piping work performed outside the building, including “retention systems”.
- All primary roof drains shall be piped internally to below grade, exit the building, and be connected to the storm sewer. MPC 4714.1101.1.
- Secondary Roof Drainage shall drain to an approved place of disposal in the form of Secondary Roof Drains installed per MPC 4714.1101 & 1102, and Minnesota State Building Code 1503.4 1-5. Secondary roof drainage must discharge onto permeable soils and cannot drain onto the sidewalk. MPC 4714.1101.1. An approved place of disposal that can be approved by the

Authority Having Jurisdiction for secondary roof drainage is in the form of secondary roof drains piped internally, down to within 18 inches of grade, through the outside wall, onto a splash block installed per MPC 1101.5.3, and laid over permeable soils of an adequate amount where saturation of the soil will not occur.

Response: These notes have been added to the Civil plan sheets.

Water Utility

Jeff Murphy/ 651-266-6276 jeffrey.murphy@ci.stpaul.mn.us

14. Per the 11/09/2017 SPR Report, show existing water services on Plan Sheet C1 or C4 to be cut off at the water main.

- 1977 Marshall Ave – 1" CP – 30 S N Marshall / 28 E E Moore
- 1973 Marshall Ave – 1" CP – 80 E E Moore

Response: This information has been added to the Civil plan sheets.

Forestry

Zach Jorgensen/651-632-2437 zach.jorgensen@ci.stpaul.mn.us

15. The existing street trees on Marshall Avenue are not consistently shown in the updated plan set (architectural and civils). Please include the trees in the site plan and indicate whether they will be removed/replaced or protected during construction.

Response: The existing street trees on Marshall Avenue have been coordinated.

Metro Transit

Berry Farrington/612-349-7378 berry.farrington@metrotransit.org

16. Consider adding a cap to the retaining wall adjacent to the sidewalk near the corner at Marshall and Moore. A cap of up to 24" deep would help protect the private landscaping from damage that could occur from people sitting on the wall.

Response: A 24" deep retaining wall cap has been added to the proposed project.

Sincerely,

POPE ARCHITECTS, INC.

Paul Hanlon
Project Architect

cc: File, Jon Schwartzman, Eric Meyers



UNION PARK DISTRICT COUNCIL

161 Snelling Avenue North, Saint Paul, MN 55104
651.645.6887 | info@unionparkdc.org | www.unionparkdc.org
An Affirmative Action, Equal Opportunity Employer

November 6, 2017

Tia Anderson
Department of Safety and Inspections
City of Saint Paul
375 Jackson Street
Saint Paul, MN 55101

Dear Tia:

The Union Park District Council Committee on Land Use and Economic Development convened a special meeting on October 30 to discuss the site plan for the proposed development at 1973/1977 Marshall Avenue. Property owner Jon Schwartzman and architect Paul Holmes presented on the project.

I have summarized the primary concerns and requests set forth by Committee and community members at the meeting. Thank you for the opportunity to share these issues with the site plan review team.

1. Building height

A primary request is to lower the overall height of the proposed five-story structure by one—or preferably two—stories. The prevailing reasons for this include the facts that:

- All of the multifamily residences in the area are a maximum of three stories; this proposed structure is much taller than other nearby buildings.
- The west elevation of the building as proposed is essentially six stories, exceeding the maximum allowable number of stories.
- There is concern that the building as submitted in the plans exceeds the maximum allowable height of 50 feet, both in building design and through the establishment of a new grade.
- In the City's Comprehensive Plan, Marshall Avenue is a medium-density residential corridor, which calls for a maximum density of 30 units per acre; the density of the proposed project is reportedly about 49 units per acre.

2. Balconies and window issues

The architect has acknowledged that the tenants of this property will be primarily—if not exclusively—college students. Accordingly, there is general consensus that the balconies should be removed from the plans for the following reasons:

- Noise impacts are of primary concern to the community. Excessive late-night noise from balcony use, exacerbated by their height, is anticipated.
- Balconies pose a safety issues as well as a greater likelihood for nuisance, such as thrown objects and urination off of them; neighbors in the area have experienced similar nuisances created by student rentals in the past.
- With the limited amount of storage provided within the building, there is concern that the balconies will become cluttered and unsightly.

The developer is also urged to replace the proposed vinyl windows with higher-quality energy-efficient non-opening windows to contain sound within the building.

3. Traffic and parking

With the number of tenants (not to mention visitors) at this property likely providing a net of at least 54 additional vehicles, a traffic study and traffic management plan are called for, focused on the Marshall and Moore intersection. With Four Seasons A+ Elementary and St. Mark's schools in the immediate vicinity, and frequent church traffic on weekends, there is a need to consider a marked crosswalk, pedestrian refuge median, rectangular rapid flashing beacon, and other types of traffic control at that intersection to ensure pedestrian safety and adequate left-hand turning movements.

There are also significant parking concerns. With 64 residents and 32 parking spaces provided, pressure on side-streets will be significant. Specifically, with respect to parking:

- There are concerns that if tenants must pay extra for off-street parking, that they will elect not to use it. Thus, the developer is urged to attach parking costs to the units.
- There are concerns about the adequacy of proposed off-street parking. For example, questions have been raised as to the legality of the separate garage structures (situated in the side yard, and violating set-back requirements) and the feasibility of the parallel spot in the main garage.
- Better bicycle parking options are also requested: outdoor bicycle parking is not secure in this area, and bicycle parking over vehicles in garage does not seem desirable.

4. Exterior appearance and finishes

There is strong sentiment that the proposed building is entirely out of character with the neighborhood. Primary requests include the following:

- The building's design should better match the historic nature of the neighborhood, with an early 20th century classic appearance.
- Recessed or set-back upper stories would reduce the monolithic appearance and break up the exterior façade; the EastRiver Apartments at 2320 Marshall was identified as an example of this.
- A primarily, if not exclusively, brick exterior would better comport with the neighborhood character; the Vintage development at 1555 Selby Avenue was identified as an example of this.
- Recessed windows with thresholds and reveals would give building design more relief.
- Higher quality and refinement of finishes overall is desired; there is a sense that student tenants will treat the property with more care if it is higher quality.

5. Other issues and concerns

Additional requests and concerns with consensus support include the following:

- A shadow analysis should be completed, and shade impacts on surrounding residences should be mitigated to the extent possible to preserve the ability to capture solar and grow vegetation.
- Trash and recycling must be adequately addressed and managed with minimal impacts to surrounding residences.
- Situating the rental management office on-site is supported, but the owner has indicated that it will also serve as the leasing office for his seven other rental properties. There is concern that this commercial-type use will be burdensomely intense and not allowed within RM2 zoning.

- The property owner must institute a robust security plan with adequate lighting, cameras, and other security measures.
- The property owner must have strong lease conditions with expectations and regulations related to quiet hours, nuisance issues, visitors, and over-occupancy.
- The contractor must share a construction management plan for demolition and development, and weekly communication on the project with designated neighbors and the district council.

There have also been concerns expressed about recent maintenance-based complaints lodged against this developer with respect other student-rental properties he owns, concerns that he has rented to students in violation of the student housing overlay, and concerns about his selection of a newly-graduated college student as the on-site manager of this housing complex.

6. Union Park District Council's ten-year plan

While likely more pertinent for the Planning Commission public hearing than for site plan review, there are numerous provisions of Union Park's Ten-Year Community Plan that apply to support the above requests. In its plan, the community has committed to:

- Preserve the well-kept, traditional feel and scale of the neighborhood (LU2), and preserve and improve the character of the neighborhood (H2).
- Ensure that new development fits within the character and scale of adjacent neighborhoods (LU2.3).
- Support multi-unit mixed-use development *in mixed-use corridors* (specifically, Marshall Avenue *between Snelling and Hamline* per LU3.2) that can accommodate higher density levels, while minimizing impacts on adjacent lower density areas, and discourage multi-unit housing uses that are incompatible with single-family residential areas (H1.1).
- Integrate historic significance into Union Park's housing and land use decision-making processes, supporting preservation over demolition (HP1) and encourage the continued use and rehabilitation of existing structures to preserve the historic character of residential and commercial districts (LU2.2).

And, the Plan mandates that "new residential construction shall be consistent with the character of the surrounding homes, while minimizing impact on the neighborhood." (H2.2a).

This project is the first major redevelopment along this stretch of Marshall Avenue in 50 years. The project's architect acknowledged that "bringing this higher density student use" will have a significant impact on the neighborhood. The City Council recently unanimously supported an interim ordinance putting a hold on such projects so that development can be done thoughtfully, in a way that meets our City's increased housing demand while preserving important historical assets and neighborhood character when possible. We request that City staff approach the evaluation of this project with those principles in mind.

Sincerely,



Julie Reiter, Executive Director, Union Park District Council

From: Meg Arnosti <arnosti.meg@gmail.com>

Sent: Wednesday, December 27, 2017 11:54:18 PM

To: Englund, Cherie (CI-StPaul); Diatta, YaYa (CI-StPaul); Johnson, Tony (CI-StPaul); dan.edgerton@stantec.com; blindeke@gmail.com; cedrick.baker@gmail.com; adejoy@esndc.org; christopher.james.ochs@gmail.com; ecr@trios-llc.com

Subject: Oppose 1973 and 1977 Marshall Avenue development

Dear Mr. Edgerton and Members of the Zoning Committee:

I urge you to oppose the development at 1973/1977 Marshall Avenue for the following reasons.

Union Park's 10-Year Community Plan calls for preserving the traditional feel of the neighborhood, discouraging multi-unit housing that is incompatible with single-family houses, and integrating historic significance into housing decisions. The existing homes at 1973 and 1977 Marshall contribute to the historic and unique character of Marshall Avenue, but the proposed apartment building would be destructive to that character.

The City Council approved a one-year development moratorium along these six-blocks of Marshall Avenue specifically to give a chance to evaluate the destruction of this sort of historic housing. On that very day, the developer squeaked in under the wire and submitted his hasty proposal to try to avoid being subject to the moratorium.

The University of St. Thomas has promised for years to supply enough student housing for its students and has failed to do so. This results in pressure to destroy the beautiful residential neighborhood forever.

The west elevation of the building as proposed exceeds the maximum allowed height of 50 feet because it is elevated on a hill from the street.

Marshall Avenue is a medium-density residential corridor in the City's Comprehensive Plan. This allows a maximum density of 30 units per acre; the density of the proposed project is 49 units per acre.

Only 32 parking spaces are proposed for 64 residents, and tenants will have to pay extra for these spots. This means that parking will overflow into already-crowded residential streets.

This development would unreasonably burden the neighboring homes, especially 1969 Marshall Avenue, and also 1985 Marshall Avenue and 1972 and 1980 Iglehart Avenue, with stormwater runoff, car noise and fumes.

Please keep in mind that once the character of Marshall Avenue changes, the charm of one of our great streets will be lost forever. Thank you for your careful consideration of this matter.

Sincerely,
Margaret Malde-Arnosti
1722 Princeton Avenue
St. Paul, MN 55105

From: pcc322 <pcc322@centurylink.net>

Sent: Wednesday, December 27, 2017 9:21:53 PM

To: Englund, Cherie (CI-StPaul); Diatta, YaYa (CI-StPaul); Johnson, Tony (CI-StPaul)

Subject: Merriam Park development.

To the Zoning Committee:

I am an active member of the congregation of the Evangelical Formosan Church at 1982 Iglehart.

I am concerned about the plan to tear down the house at the NE corner of Marshal and Moore.

I arrive early on Sunday before most of the congregation, and at that early hour, I can't always find a convenient place to park. Those who arrive later may have to walk nearly a block.

We have a couple of handicapped members, and though there isn't any designated handicapped parking places, most of us try to leave a convenient spot or two.

I know from a brief encounter that the tight parking situation on Sunday mornings does irritate some of the neighbors, and I sympathize with them.

So... As it is there is barely enough parking in the neighborhood as it is. Adding more automobiles to the on street parking will cause problems for both the residents in the neighborhood and those who want to park on the street just once or twice a week.

I urge you to withhold approval of the development plans for that neighborhood.

James A Willmore
18795 Partridge Circle
Eden Prairie. MN 55346

From: Helen Holtti <hholtti@yahoo.com>
Sent: Thursday, December 28, 2017 9:58 AM
Subject: Moore and Marshall apartments
To: Englund, Cherie (CI-StPaul) <cherie.englund@ci.stpaul.mn.us>

Cherie,

I am writing to express my opposition to the apartment on Marshall and Moore. My daughter lives at 1999 Iglehart- just around the corner. I am a senior citizen and come over to visit my daughter often. On her side of the block is 11+ students living on the odd side of the block. Parking near her home is already practically impossible. If 50+ more cars are added - I will be carrying groceries for blocks!!! The students aren't aware of not blocking crosswalks or leaving a space in front even though they have been asked to do so. Plus 14+ busses go down Moore and Iglehart every school day making the streets very busy and congested. If a bus is traveling down the street you have to wait for it to pass - there is only room for one vehicle at a time because there are many cars on both sides of the street.

Please pass my email on to all members

Helen Holtti

651-280-7509

Sent from my iPhone

From: Madison Pierce <madison27pierce@gmail.com>
Sent: Thursday, December 28, 2017 11:41:34 AM
Subject: Marshall & Moore Apartment Development

Hello,

I'm a resident at 1990 Marshall avenue (across the street from the proposed development) and I oppose this project. While increased density in the neighborhood is essential, the units are simply unaffordable. We don't need to add to the luxury apartment bubble we currently see in the Twin Cities. It's not hard to see why \$800/bedroom/month is unrealistic for most people.

I would support the project if the units were affordable. I don't even care what they look like! I'm not annoyed by students being around the neighborhood! I'm not concerned about noise or parking and I don't even mind living across the street from a development project. I support density but I cannot support the construction of more luxury apartments.

Madison Pierce
1990 Marshall Avenue

From: Emma Kopp <em.kopp94@gmail.com>

Sent: Thursday, December 28, 2017 11:53:28 AM

To: Englund, Cherie (CI-StPaul); Diatta, YaYa (CI-StPaul); Johnson, Tony (CI-StPaul); dan.edgerton@stantec.com; blindeke@gmail.com; cedrick.baker@gmail.com; adejoy@esndc.org; christopher.james.ochs@gmail.com; ecr@trios-llc.com; Henningson, Samantha (CI-StPaul); #CI-StPaul_Ward4; #CI-StPaul_Ward7

Subject: Statement of Opposition to the Marshall & Moore Apartments

Dear Zoning Committee,

I am writing to urge you to join me and many others in opposing construction of the proposed Marshall and Moore Apartments.

First and foremost, Mr. Schwartzman's plans do not, in any way, support affordable housing. \$800 a room, for student or otherwise, is not affordable housing.

While I understand and feel the weight of a growing housing crisis in the Twin Cities, and namely our Merriam Park community, this proposition is not the correct way to go about it. The construction of apartments like the Marshall and Moore Apartments do nothing to conserve the history and charm of our neighborhood and will lead directly to more gentrification of the area.

Developing these apartments is a disgusting corporate debasement of our community and I urge you to oppose the Marshall and Moore Apartments project.

With respect,
Emma Kopp
1696 Marshall Ave
763-412-0704

MOHRMAN, KAARDAL & ERICKSON, P.A.

ATTORNEYS AND COUNSELORS AT LAW

150 SOUTH FIFTH STREET
SUITE 3100
MINNEAPOLIS, MINNESOTA 55402

ERICK G. KAARDAL

TELEPHONE: 612/341-1074
FACSIMILE: 612/341-1076
WRITER'S E-MAIL: KAARDAL@MKLAW.COM

December 21, 2017

Via U.S. Mail & E-mail

Mr. Yaya Diatta
Zoning Administrator
City of Saint Paul
Department of Safety and Inspections
375 Jackson Street, Suite 220
Saint Paul, MN 55101-1806

Re: "Marshall & Moore Apartments"
1973/1977 Marshall Avenue lots (Revised SPR File # 17-206385)

Dear Mr. Diatta:

As previously noted, I represent a number of Merriam Park residents and the Church of St. Mark, St. Paul, Minnesota, in this matter. While we were disappointed that the original Marshall & Moore Apartments site plan review application submitted on October 18, 2017, was not rejected by the City as incomplete, my clients would like to convey their appreciation to City staff for their expert and thorough site plan review process.

We have now reviewed the revised site plan review application (the "Revised Application") that the Saint Paul Department of Safety & Inspections received, on December 14, 2017, for the proposed multiple-family residential project named "Marshall & Moore Apartments" (the "Project") on the combined 1973/1977 Marshall Avenue lots (SPR File # 17-206385). I am writing because the Revised Application, as submitted, is defective and does not fundamentally comply with prescribed City of Saint Paul medium-density RM2 multiple-family residential zoning dimensional standards, Chapter 63 Zoning Code – Regulations of General Applicability, and certain federal and state statutes currently in effect for these two City lots. The City previously informed the applicant that he would not be permitted to seek any zoning or other variances from the City for the Project, in light of interim Ordinance 17-54 (moratorium) now in effect over west Marshall Avenue between Wilder and Wheeler streets. Accordingly, the Revised Application for SPR File # 17-206385 should be denied, and the City should inform the applicant of its denial of the Revised Application within the timeframe established pursuant to Minn. Stat. § 15.99, Subd. 3. In the event DSI does not deny the Revised Application and instead submits it to the Planning Commission, we respectfully request that you include this letter and

exhibits with the Zoning Committee Staff Report you deliver to the Planning Commission and its Zoning Committee.

Additionally, the Revised Application is defective because several key components of the Project, if built, would create nuisance for and unreasonably affect abutting properties and the owners of such properties. Further, the applicant has not proposed reasonable protection provisions for adjacent and neighboring properties to address unreasonable interference and nuisance, such as adequate surface water drainage, sound and light buffers, preservation of views, light, and air, and those aspects of design, which may have substantial effects on neighboring uses. For example, the Project proposes approximately 3,100 square feet (nearly 22% lot coverage) of paved driveway in the side yard within four feet of the shared property line with the adjacent property, 1969 Marshall Avenue. 1969 Marshall is only two feet from this shared property line. While driveways in side yards are not unusual for single-family residential dwellings, a side-yard driveway to access nine parking spaces (eight enclosed and one surface space) in a residential zoning district, with traffic, car and garage-door noises, lights, and exhaust during all times of the day, creates an unreasonable, noncompliant interference and unacceptable nuisance for the abutting properties and owners. As the Project is proposed, there is no way to eliminate this nuisance.

The Revised Application still does not provide complete information, such that a comprehensive review of the proposed development can be completed, or one may determine whether the Project, as designed, could actually be built. City requirements are clear that a complete application must include specific information regarding not only existing conditions at the site, but also information about the proposed development and its likely impact on surrounding areas. In this case, the applicant failed to provide key information including, but not limited to specific information requested by Tia Anderson in her November 8, 2017, Site Plan Review Report and certain structural drawings. According to the Revised Application, the building will be constructed with a concrete garage structure, with a portion underground. From the sketches on the Revised Application's Garage Plan and Building Section sheet, which is not drawn to scale, it is impossible to determine whether the building's parking structures will accommodate load requirements and fire separation codes. The architectural documents submitted do not establish accurate parking structure clear heights directly below and outside the building, slab and beam depths, and lighting requirements, and likely do not reflect designed column locations further reducing the underground parking count and maneuvering lane widths.

In addition, the defective and incomplete nature of the Revised Application is illustrated by the attached exhibits A through F. In summary, the Project, as designed:

1. Exceeds the maximum height permitted in RM2 zoning districts;
2. Requires major variances to encroach in the rear and interior side-yard setbacks for an off-street parking facility;

3. Requires major variances to encroach in the rear and interior side-yard setbacks for the “underground” parking structures;
4. Does not meet minimum off-street parking requirements;
5. Requires major variances to exceed the maximum lot coverage in RM2 zoning districts from 35% to 54.1%;
6. Requires major variances to exceed the maximum density in RM2 zoning districts;
7. Requires major variances to reduce the minimum lot area per dwelling from 1,500 square feet to 1,248 square feet;
8. Requires major variances to reduce the minimum off-street parking from 30 to 20 spaces;
9. At 49 units per acre, proposes a density standard that materially exceeds St. Paul’s *Generalized 2030 Future Land Use Comprehensive Plan* for a Residential Corridor, such as west Marshall Avenue;
10. Does not comply with requirements set forth in the *Union Park Community Plan*;
11. Does not preserve certain unique geologic, geographic, or historically significant characteristics of the City;
12. Does not adequately protect adjacent and neighboring properties through reasonable provision for such matters as surface water drainage, sound and light buffers, preservation of views, light, and air, and those aspects of design, which may have substantial effects on neighboring uses;
13. Does not arrange the Project’s building, uses, and facilities in order to assure abutting properties and property occupants will not be unreasonably affected;
14. Does not provide safety and convenience of both vehicular and pedestrian traffic both within the site and in relation to access streets, including traffic circulation features, the locations and designs of entrances and exits, and parking areas within the site;
15. Does not demonstrate availability and capacity of storm and sanitary sewers, including solutions to any drainage problems in the area of the Project;
16. Proposes insufficient landscaping, fences, wall, and parking necessary to meet the City’s residential zoning objectives; and
17. Does not provide site accessibility in accordance with provisions of the Americans with Disabilities Act, including parking spaces and accessible routes.

Additional deficiencies with the Revised Application include, but are not limited to:

1. As noted above, structural drawings were not submitted for review;
2. Snow storage still appears insufficient for the proposed impervious surface area;

3. No elevator equipment or hydraulic pump room (approximately 5' x 10') is shown adjacent to the elevator shaft in the parking garage or first floor; if installed in the parking garage, would potentially interfere with parking spaces and access; and
4. Trash and recycling area still appears insufficient for the proposed minimum occupancy of 61 residents.

The City has demonstrated its long-term commitment to ensuring that redevelopment along west Marshall Avenue occurs in a careful, well-planned, and zoning compliant manner through the City Council's unanimous approvals of: (1) Resolution 17-1582, which authorized the zoning study of land use and land use classifications along Marshall Avenue between Mississippi River Boulevard and Hamline Avenue and (2) Ordinance 17-54, an interim ordinance pursuant to Minn. Stat. § 462.355, Subd. 4, pending completion of the West Marshall Avenue Study, specifically between Wilder and Wheeler streets, to preserve the status quo and protect the integrity of the West Marshall Avenue Study's recommendations. Approving an application, such as this one, would directly contravene the important planning commitment that was made by the City.

We look forward to your confirmation that the Revised Application has been denied, with notice to the applicant within the City's timeframe pursuant to Minn. Stat. § 15.99, Subd. 3.

Sincerely,



Erick G. Kaardal

EGK/mg

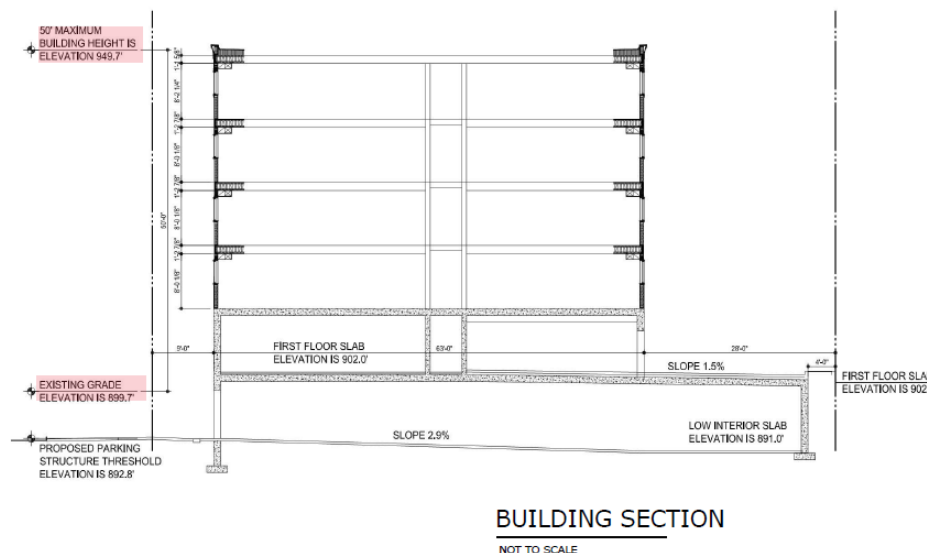
Enclosures

cc: Samuel J. Clark, City Attorney
Ricardo X. Cervantes, Director, Department of Safety and Inspections
Wendy Lane, St. Paul Zoning Administrator, Department of Safety and Inspections
Lawrence R. Zangs, Site Plan Review Coordinator, Department of Safety and Inspections
Tia Anderson, Senior City Planner, Department of Safety and Inspections
Council President Russ Stark, Ward 4 Councilmember
Tammera R. Diehm, Winthrop & Weinstine

Marshall & Moore Apartments Project Exceeds the Maximum Height Permitted in RM2 Zoning

The maximum height in the RM2 Medium-Density Multiple-Family Residential District is 50' above the “established grade” per Code Section 60.203.-B and per Table 66.231 in the Zoning Code. The proposed height is shown to be 50.0' from the established grade to the roof deck, with no margin for error. Project’s Building Section Sheet (12/14/17) shows established grade to be 899.7' and the roof deck height is 949.7'. The garage entry is stated as 892.0'

The “underground” garage structure clearances are insufficient to meet the minimum required for the building code (minimum clearance for non-handicap parking structures is 7'-6”). Additionally, concrete garage structure floor depths appear understated. Once true scaled plans are shown, the building will prove to exceed the maximum height permitted in RM2 Zoning.



Marshall & Moore Apartments Project Requires Major Variance to Encroach in the Interior Side Yard Setbacks for Off-Street Parking Facility

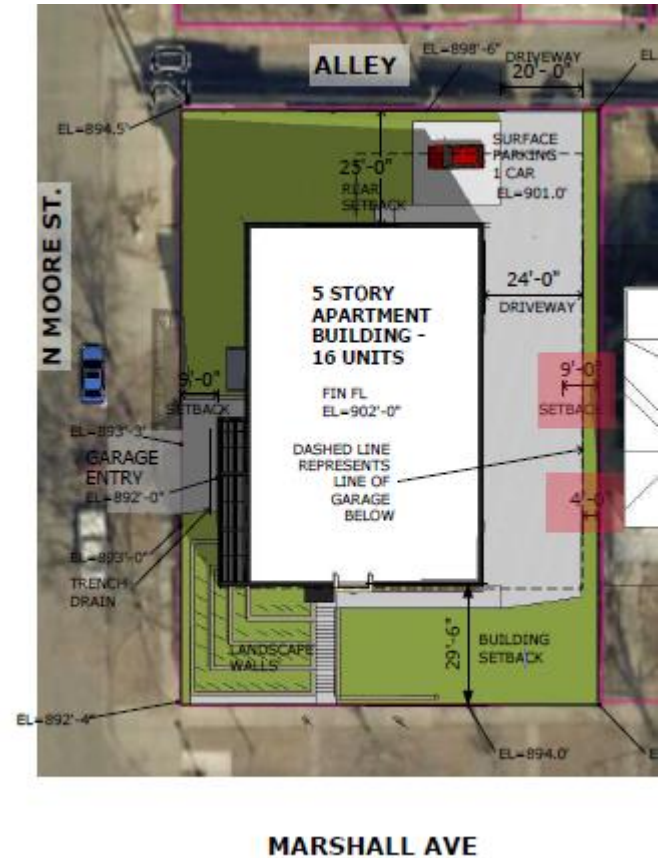
Ordinances:

Section 60.217. – P. Parking facility, off-street. All areas, spaces and structures designed, used, required or intended to be used for the parking of more than three (3) motor vehicles. This definition is intended to include adequate driveways, accessways, parking bays, garages of a combination thereof, but does not include public roads, streets, highways and alleys.

Section 63.312. – Setback. Except as otherwise provided in section 66.442(a) or section 66.431(b) off-street parking spaces shall not be within a required front or side yard and shall be a minimum of four (4) feet from any lot line.

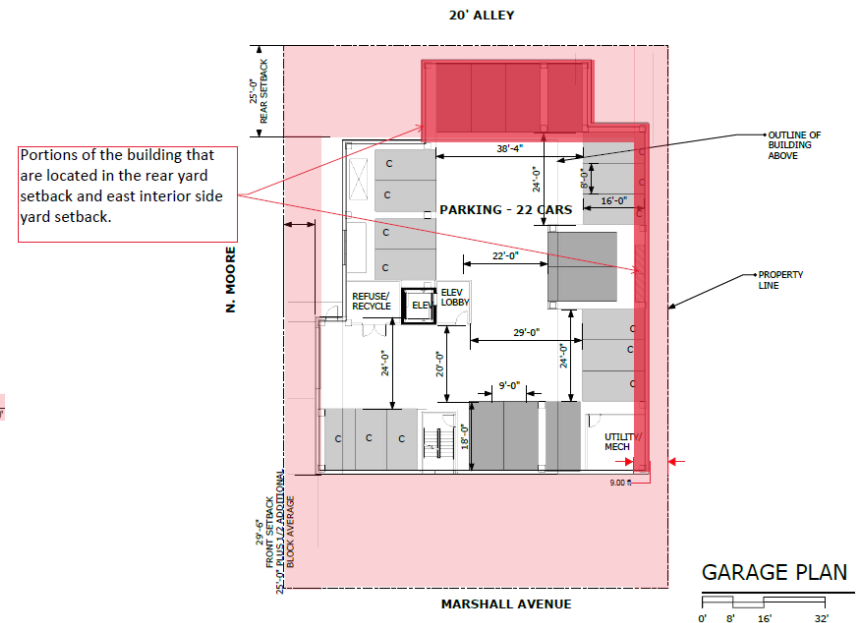
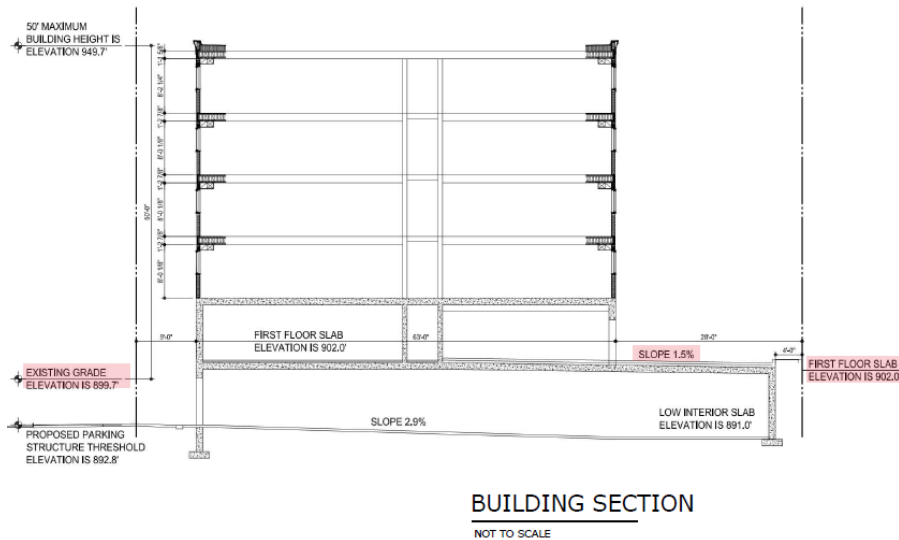
Section 63.106. – Projections into yards. There are no exemptions for parking facilities in the required interior side yard.

The proposed above-ground parking facility, accessed from the public alley, is located 4' from the east interior side property line and requires a major variance to encroach into the east interior side setback, required to be 9' from the property line.



Marshall & Moore Apartments Project Requires Major Variances to Encroach in the Rear and Interior Side Yard Setbacks for Parking Structure

The proposed first floor slab elevation is 902' according to the Building Section plan (12/14/17). The established grade elevation is 899.7'. Therefore, the "underground" parking structure is 2.3' taller than established grade nearest the structure and 1.94' above established grade at the eastern edge (1.5% slope). As such, the structure fails to comply with the side and rear yard building setbacks required for the above-grade portions of the principal structure.



The Marshall & Moore Apartments Project Requires Major Variance to Increase the Maximum Lot Coverage

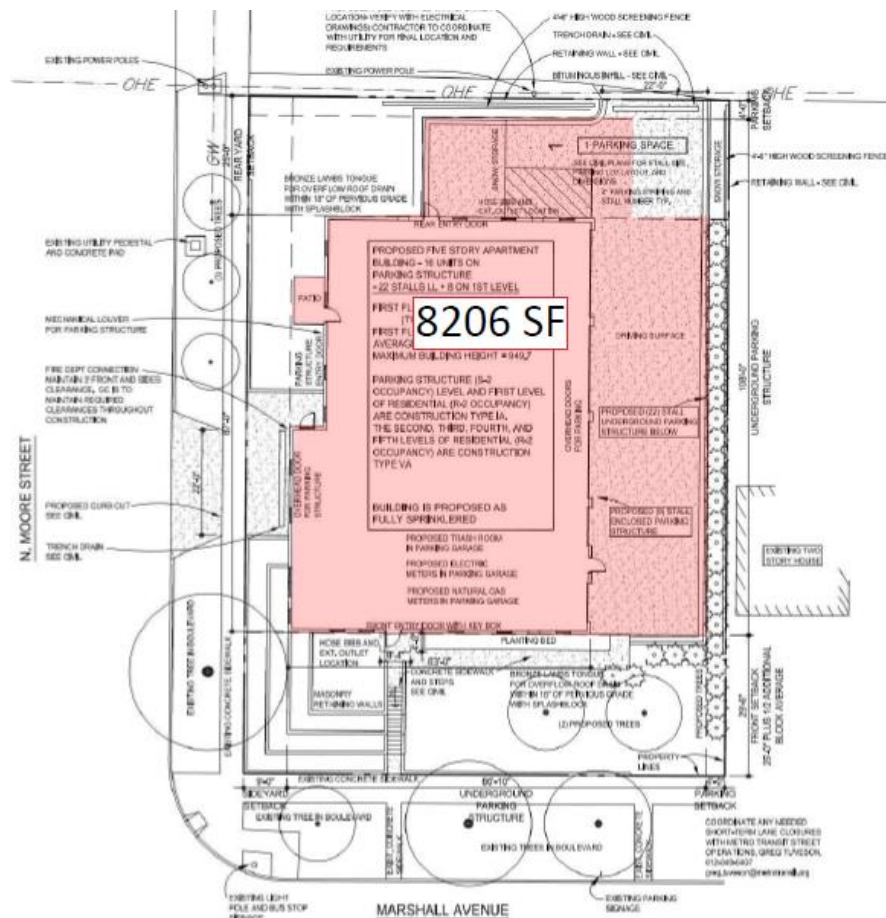
Ordinances:

Section 66.232. In residential districts, principal buildings shall not cover more than thirty-five (35) percent of any zoning lot.

The Marshall & Moore Apartments Project:

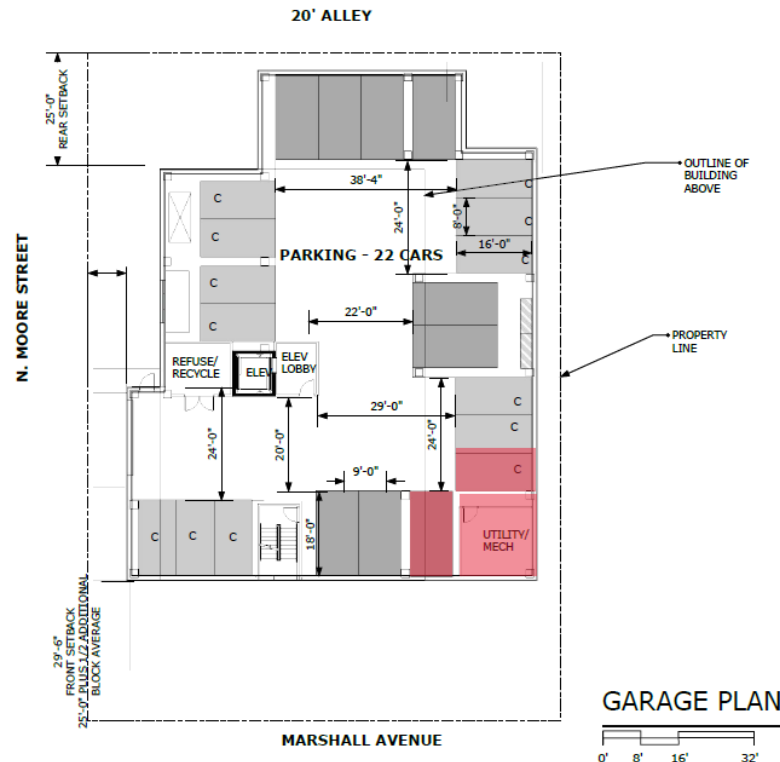
As previously demonstrated, the “underground” parking structure is 2.3’-1.94’ taller than established grade (see Exhibit C). Therefore, the entire footprint of the structure, including the “underground” parking structure, shall be included in the calculation for lot coverage.

8,206 SF / 15,169 SF = **.541 OR 54.1% of the total site area**



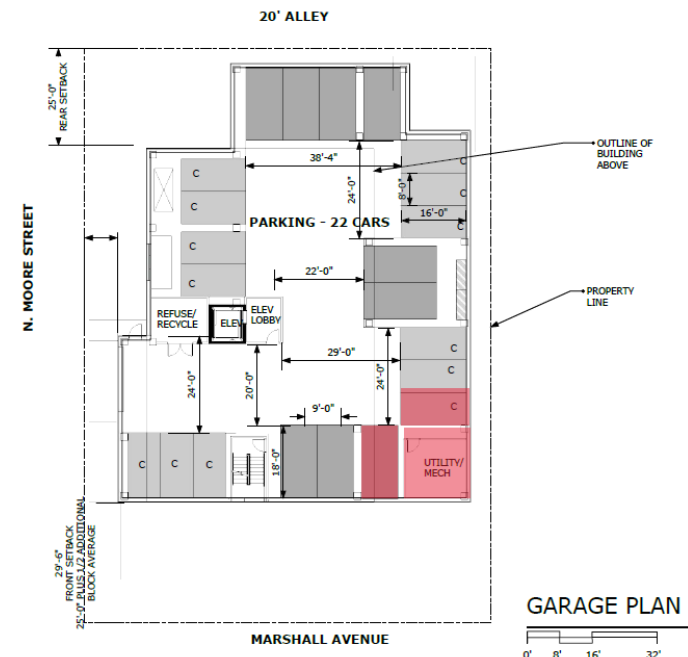
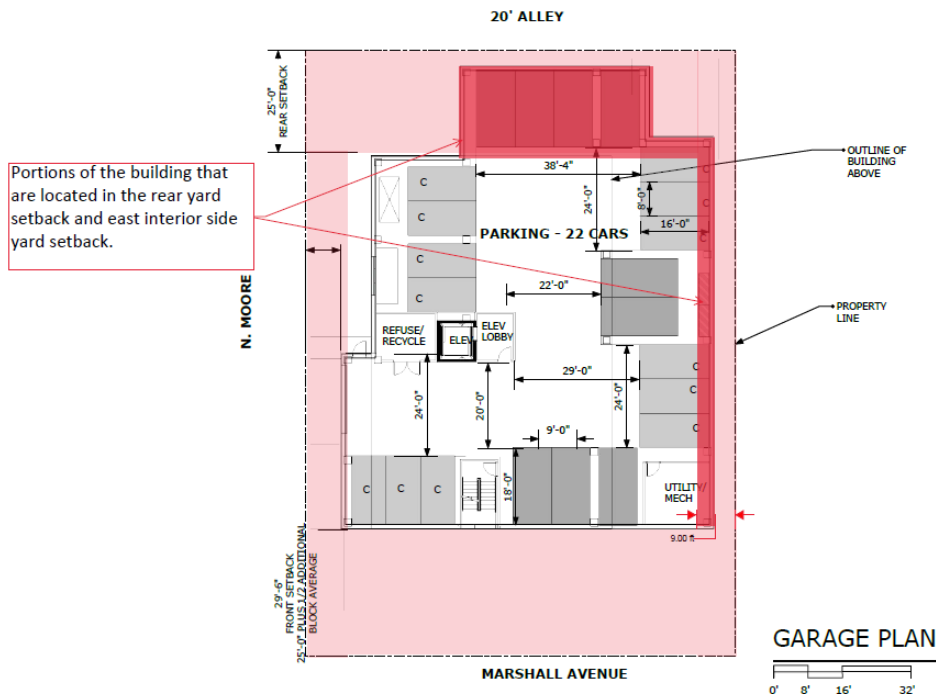
Marshall & Moore Apartments Project Does Not Meet Minimum Off-Street Parking Requirements

The proposed utility and mechanical room is located in the southeast corner of the “underground” parking structure. This room is flanked on both sides with parking spaces, rendering the room inaccessible. This will result in the loss of 1 parking stall to accommodate the required access to the utility and mechanical room.



Marshall & Moore Apartments Project Does Not Meet Minimum Off-Street Parking Requirements

As previously demonstrated, the “underground” parking is above the established grade (see Exhibit C) and 10 stalls do not comply with building setbacks and the utility and mechanical room is inaccessible (see Exhibit E), which results in the loss of 1 more parking stall. Only 20 of the proposed 31 parking stalls would not require a major variance, as shown in the Project application. A minimum of 30 parking stalls are required for 13 – 4-bedroom units and 3 – 3-bedroom units. The Project requires a variance to reduce the minimum off-street parking from 30 spaces to 20 spaces.



Marshall & Moore Apartments Project Requires Major Variance to Exceed the Maximum Density in RM2

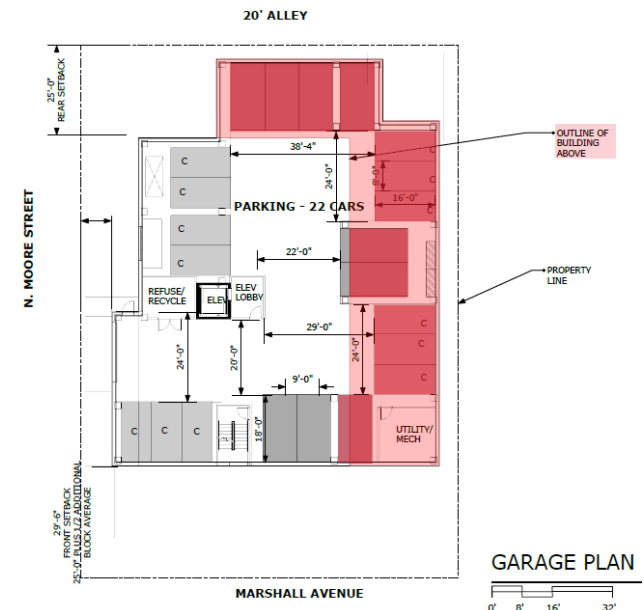
Ordinance:

Note (c) to Zoning Code Table 66.231 states parking spaces within a multiple-family structure or otherwise completely underground qualify for the density bonus.

The Project application requests a density bonus for 30 enclosed stalls. The “underground” parking structure is not located within the envelope of the multiple-family structure or otherwise completely underground (see Exhibit C). Therefore, the 13 spaces in the “underground” parking garage shown below do not qualify for a density bonus. This determination is further supported by the interpretation provided by DSI on November 26, 2014, regarding 2136-2144 Grand Avenue.

The position of the Zoning Administrator is that a literal reading of the Zoning Code as well as a common sense interpretation says that your proposed garages would not qualify for the lot area bonus:

- The Zoning Code defines Structured Parking as “Off-street parking that is placed within a ramp, deck, underground, enclosed building, or tuck-under garage.” It also says a lot area bonus can be given for “parking spaces within an above-ground parking structure, except for the top level.” (Emphasis added.) So even if it was determined that your garages were Parking Structures, the second floor are not proposed to be used for parking and so they cannot be considered as part of a “Parking Structure” under the definition. That leaves the first floor parking as the only (and therefore top) level of parking and this clearly cannot be counted for the lot area bonus.
- The principal underlying the lot area bonus is that a residential development that puts land needed for parking to use for additional levels of either parking or housing warrants a bonus of an additional units. Essentially, when the land area is used multiple times, more of the property remains for landscaping, storm water infiltration, and light and air for the neighbors. Your proposed garages do not meet this standard.



Marshall & Moore Apartments Project Requires Major Variance to Exceed the Maximum Density in RM2

The Project application requests a density bonus for 30 enclosed stalls. 13 spaces in the “underground” garage do not qualify for a bonus as they do not meet the standards in the Notes to Table 66.231 (c). One additional stall in the “underground” parking structure must be eliminated due to accessibility conflicts with the utility/mechanical room (see Exhibit E). As such, only 16 spaces would qualify for the bonus. The reduced bonus results in a maximum of 13 apartment units allowed; 3 units fewer than proposed.

LOT AREA DATA (See Project Site Plan)

13 Dwellings @ 1,500 SF	TOTAL	19,500 SF
 LOT DIMENSIONS FR AREA =	100 X 141 SF	14,171 SF
1/2 ALLEY=	10 X 99.8 SF	998 SF
16 ENCLOSED STALLS X 300	(BONUS)	<u>4,800 SF</u>
		19,969 SF

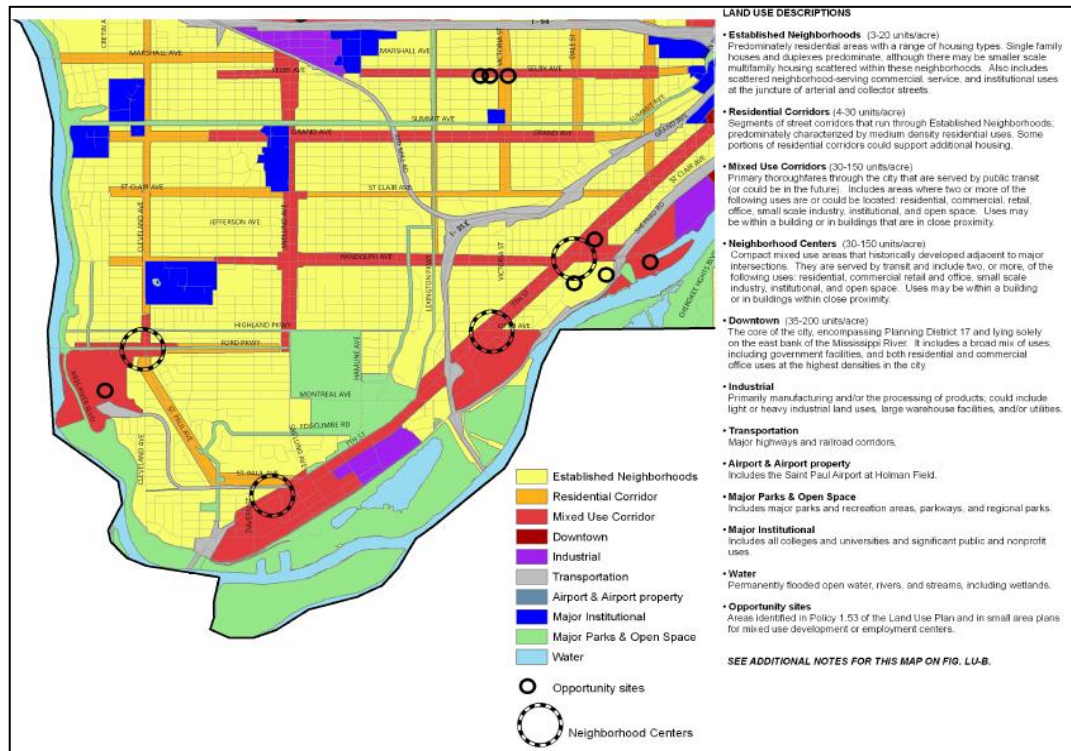
19,969 SF / 16 Dwelling Units = 1,248 SF/DU where 1,500 SF/DU is Required

Analysis of Findings Required for Site Plan Review

1. The city's adopted comprehensive and development or project plans for sub-areas of the city.

At 49 units per acre, Marshall & Moore Apartments density exceeds the St. Paul's *Generalized 2030 Future Land Use Comprehensive Plan*. Marshall Avenue is a designated Residential Corridor in the St. Paul's *Generalized 2030 Future Land Use Comprehensive Plan*. The guided density range for Residential Corridors is 4-30 dwelling units per acre and expressly stated in Strategy 1.1 and the Land Use Table.

FIGURE LU-K. GENERALIZED 2030 LAND USES, SOUTHWEST SECTOR



Analysis of Findings Required for Site Plan Review

1. *The city's adopted comprehensive and development or project plans for sub-areas of the city (continued).*

Strategy 1.1 from the Generalized 2030 Future Land Use Comprehensive Plan

LU 1.1 Guide the development of housing in Established Neighborhoods, commercial areas within Established Neighborhoods, and in Residential Corridors. This policy is intended to provide for the development of housing in Established Neighborhoods, Residential Corridors and adjacent commercial areas consistent with the prevailing character and overall density of these areas. The density goals are residential development of 3-20 dwelling units per acre in Established Neighborhoods and residential development of 4-30 dwelling units per acre in Residential Corridors and adjacent commercial areas.

LU 1.2 Permit high density residential development in Neighborhood Centers, Mixed-Use Corridors, the Central Corridor, and Downtown. Residential development in the Central Corridor is addressed on page LU-14. For Mixed-Use Corridors, the City should permit residential development of 30-150, including Neighborhood Centers within Mixed-Use Corridors. Residential development in Downtown should be permitted at a density of 35-200 dwelling units per acre.

Residential corridors are specifically guided for medium density. Conversely, high density residential development is guided for Neighborhood Centers, Mixed-Use Corridors, the Central Corridor, and Downtown. For Mixed-Use Corridors such as Snelling, University, and vast segments of Selby, Grand, and Randolph Avenues, and West 7th Street, the City should permit residential development of 30-150 dwellings/acre. Likewise, Neighborhood Centers and Mixed-Use Corridors would expect the same density range (30-150 dwellings/acre). 49 dwellings per acre, as designed in the Marshall & Moore Apartments, is consistent with development density in Downtown, which allows 35-200 dwellings/acre. **The proposed density of 49 units per acre is inconsistent with the St. Paul Comprehensive Plan for a Residential Corridor, including Marshall Avenue.**

Analysis of Findings Required for Site Plan Review

1. The city's adopted comprehensive and development or project plans for sub-areas of the city (continued).

FIGURE LU-Q. SAINT PAUL RESIDENTIAL USES BY 2030 LAND USE CATEGORY

From Saint Paul Land Use Table in 5-Year Stages

Saint Paul Residential Uses by 2030 Land Use Category

Residential Land Uses	Min Density (units/acre)	Max Density (units/acre)	Total acres, 2030	Percent of land that is residential	Residential acres, 2030*	Gross acres, 2010-2030	Percent of residential acres projected to redevelop, 2010-2030	Net Residential acres to redevelop			Net new households, low buildout	Net new households, high buildout
								2010-2020	2020-2030	2010-2030		
Established Neighborhood	3	20	17,465	86%	15,020	-2.0	11.8%	-0.1	-0.1	-0.2	-4	-1
Residential Corridor	4	30	1,523	76%	1,157	-109.0	10.1%	-4.2	-4.2	-8.4	-251	-33
Mixed Use Corridor (Include NCs)	30	150	3,423	72%	2,465	1,525.0	13.3%	48.7	97.4	146.0	4,381	21,905
Downtown	35	200	326	75%	245	41.0	10.0%	1.0	2.1	3.1	108	615
Total			22,737		18,886	1,455.0		45.4	95.1	140.5	4,234	22,486

*NCs = Neighborhood Centers

* Residential acres for Established Neighborhood include the Met. Council-defined categories of Single Family Detached, Single Family Attached, Multifamily, Mixed Use Residential and Undeveloped. Residential acres for the remaining 3 land uses includes these categories as well as Mixed Use Commercial, and Retail and Other Commercial. Downtown includes all of the previous categories as well as Office.

Households Forecast (by Metropolitan Council)

	2000	2010	2020	2030
Housing units (households)	112,109	120,000	127,000	133,000

17,912 households forecasted citywide from 2010-30

September 29, 2009

The density range for Residential Corridors is emphasized as 4-30 dwelling units /acre, according to Figure LU-Q, St. Paul Residential Uses by 2030 Land Use Category in the St. Paul *Generalized 2030 Future Land Use Comprehensive Plan*. These categories are established to ensure the forecasted household projections by the Metropolitan Council have been met. The Comprehensive Plan refers only to density bonuses for affordable housing, but not enclosed parking. Therefore, it is not possible to inflate the lot area, based on a zoning code bonus. **The proposed density is 49 dwelling units per acre and is inconsistent with the St. Paul Comprehensive Plan.**

Analysis of Findings Required for Site Plan Review

1. *The city's adopted comprehensive and development or project plans for sub-areas of the city (continued).*

Union Park Community Plan (2016)

Land Use and Economic Development

Land Use Objectives and Strategies

LU2. Preserve the well-kept, traditional feel and scale of the neighborhood.

LU2.1 Maintain and establish zoning that preserves lower-density, single-family homes and duplexes outside of mixed-use corridors.

LU2.2 Encourage the continued use and rehabilitation of existing structures, districts, and landscapes to preserve the historic character of residential and commercial districts.

LU2.3 Ensure that new development fits within the character and scale of adjacent neighborhoods.

LU2.4 Preserve and increase the number of trees and green spaces within the neighborhoods and within new development; promote the creation of pocket parks, community gardens, and other public and public-private spaces.

The *Union Park Community Plan* land use objectives and strategies are intended to preserve the feel and scale of the neighborhood. Further, this Plan prioritizes the preservation of lower-density uses outside of Mixed-Use Corridors. Marshall Avenue is a Residential Corridor, not a Mixed-Use Corridor. Further, rehabilitation of existing structures to preserve the historic character is a strategy to ensure long-term compatibility with the surrounding neighborhood. Finally, LU2.3 states that new development shall fit within the character and scale. The proposed height is 20 feet/2-stories taller than structures along Marshall Avenue and will be over 56 feet above the public sidewalk. **The Project is inconsistent with the LU2. Land Use Objectives and Strategies in the *Union Park Community Plan*.**

Analysis of Findings Required for Site Plan Review

1. *The city's adopted comprehensive and development or project plans for sub-areas of the city (continued).*

Union Park Community Plan (2016)

Housing

Housing Objectives and Strategies

H1. Preserve Union Park's pedestrian-scale neighborhoods, while promoting a range of housing types and affordability to meet the needs of people at different life stages with different housing needs.

H1.1 Support multi-unit mixed-use development in mixed-use corridors that can accommodate higher density levels, while minimizing impacts on adjacent lower density areas, and discourage multi-unit housing and retail uses that are incompatible with single-family residential areas.

H1.2 Support efforts to develop a wide range of housing affordability levels, promoting more affordable housing along major transit routes including Snelling Bus Rapid Transit and the Green Line Light Rail line.

H1.3 Support housing development designed to promote pedestrian, bicycle, and public transit activity.

H1.5 Encourage owner occupation of single-family and multi-family homes.

The *Union Park Community Plan* housing objectives and strategies are intended to preserve the pedestrian-scale of the neighborhood and providing a range of housing types and affordability. The Plan mandates minimizing impacts on lower density areas and uses incompatible with single-family residential areas, outside of Mixed-Use Corridors. The proposed height is over 20 feet/2 stories taller than structures along Marshall Avenue and will be over 56 feet from the public sidewalk. The density is 49 units per acre, which exceeds all surrounding residential densities in the area. Further, the Plan encourages a range of affordability – there are no affordable units proposed. **The Project is inconsistent with H1. Housing Objectives and Strategies in the *Union Park Community Plan*.**

Analysis of Findings Required for Site Plan Review

1. *The city's adopted comprehensive and development or project plans for sub-areas of the city (continued).*

Union Park Community Plan (2016)

Housing

Housing Objectives and Strategies

H2. Preserve and improve the character and maintenance of Union Park's neighborhoods for the next 10 years and beyond.

H2.2 Encourage rehabilitation of existing housing stock.

H2.2a New residential construction shall be consistent with the character of the surrounding homes, while minimizing impact on the neighborhood.

H2.3 Identify methods to encourage property owners to enhance energy efficiency of homes and rely more upon renewable energy sources.

H2.4 Develop incentives that encourage resident upkeep of structures and landscaping.

H2.5 Study methods to hold absentee property owners more accountable for properties (i.e., through imposing a requirement that local caretaker contact information be filed with the City).

The *Union Park Community Plan* housing objectives and strategies are intended to preserve and improve the character and maintenance of the neighborhood. The Plan mandates minimizing impacts on the neighborhood and stating that new construction shall be consistent with the character of the surrounding homes. The exterior materials are predominately fiber cement board and metal panel – exteriors not found in the neighborhood. Brick is the primary exterior material found on all existing multiple-family dwellings in the immediate area. **The Project is inconsistent with the H2. Housing Objectives and Strategies in the *Union Park Community Plan*.**

Analysis of Findings Required for Site Plan Review

1. *The city's adopted comprehensive and development or project plans for sub-areas of the city (continued).*

Union Park Community Plan (2016)

Housing

Housing Objectives and Strategies

H3. Recognize and accommodate student-housing needs while respecting the rights and concerns of all community stakeholders (students, families, colleges, landlords, businesses, etc.).

H3.1 Develop incentive programs that foster responsible student-renters and responsible landlords.

H3.2 Engage the University of St. Thomas, Macalester College and Concordia University in an effort to clearly define and accommodate their current and anticipated student-housing needs.

H3.3 Explore ways to engage and educate the community about the needs, rights, responsibilities, and concerns associated with student housing.

H3.4 Explore ways to promote college and university student involvement in community affairs.

H3.5 Prioritize the development of multi-unit student housing in mixed-use corridors over the expansion of single-family rental units in traditional neighborhoods.

The *Union Park Community Plan* housing objectives and strategies recognize and accommodate student-housing needs, while respecting the rights and concerns of all community members. The Plan prioritizes new development of multi-unit student housing on mixed-use corridors over the expansion of single-family rental units in traditional neighborhoods. Marshall Avenue is a Residential Corridor, not a Mixed-Use Corridor. **The Project is inconsistent with the H3. Housing Objectives and Strategies in the *Union Park Community Plan*.**

Analysis of Findings Required for Site Plan Review

1. *The city's adopted comprehensive and development or project plans for sub-areas of the city (continued).*

Union Park Community Plan (2016)

Historic Preservation

HP1. Integrate historic significance into Union Park's housing, environmental, land use, and economic development decision-making processes, supporting preservation over demolition.

HP2. Identify, evaluate, designate, and preserve historic resources in the District.

HP2.1 Coordinate with the City's Heritage Preservation Commission and support and/or implement a survey to identify and evaluate the District's historic resources, including buildings, structures, objects, archaeological sites, districts, and landscapes, drawing on available funding sources such as the state Arts and Culture Heritage Fund.

HP2.2 Support the ongoing survey of historic resources and consider and engage the community on the designation of historic resources, such as buildings, structures, objects, archaeological sites, historic districts, and landscapes as Saint Paul heritage preservation sites or historic districts.

HP2.3 Promote ongoing preservation and continued use of all designated sites in the Union Park District.

HP2.4 Collect and inventory information on the District's history, including historical books and articles, information about historic buildings and businesses, and biographical information on significant people who have lived in the District.

The two existing structures are potential historic resources integral to establishing a Marshall Avenue or Merriam Park historic district in the future. The demolition of these resources will impact the harmonious and continuous pattern of historic structures along this corridor. **The Project is inconsistent with the HP2. Historic Preservation Objectives and Strategies in the *Union Park Community Plan*.**

Analysis of Findings Required for Site Plan Review

2. *Applicable ordinances of the City of St. Paul.*

See Exhibits A-F analyses of the following major variances:

- Major variance to increase the maximum permitted height in the RM2 District.
- Major variance to reduce the required east interior side yard setback for the parking facility from 9' to 4'.
- Major variance to reduce the required rear yard setback for the proposed “underground” parking structure from 25' to 4'.
- Major variance to reduce the required east interior side yard setback for the proposed “underground” parking structure from 9' to 4'.
- Major variance to increase the maximum lot coverage from 35% to 54.1% of the total site area.
- Major variance to reduce the minimum lot area per dwelling from 1,500 SF to 1,248 SF.
- Major variance to reduce the minimum off-street parking from 30 to 20 spaces.

3. *Preservation of unique geologic, geographic or historically significant characteristics of the city and environmentally sensitive areas.*

1977 Marshall Avenue, constructed in 1906:

- Architecturally significant structure, designed by Charles W. Beuchner (worked for renowned St. Paul architect Clarence H. Johnston); Beuchner’s notable commissions include three in use today on University Avenue:
 - Raymond/University Historic District (Specialty Building [1908] at Raymond and Carleton Place Lofts [1909]); and
 - Fire station at St. Albans Street (1908)

Analysis of Findings Required for Site Plan Review

3. *Preservation of unique geologic, geographic or historically significant characteristics of the city and environmentally sensitive areas (continued).*

1977 Marshall Avenue, constructed in 1906 (continued):

- Built for industrialist Albert A. Fry & wife, Agnes Loudon Fry (owned Minneapolis factory for the Loudon Machinery Co.)
- Original garage/carriage house still intact.
- Original exterior wood siding and intricate architectural detail and leaded glass windows largely intact.
- Interior has later modifications with some original architectural features intact.

1973 Marshall Avenue, constructed in 1906:

- Harry Metcalfe house.
- Architect: not listed on the original building permit.
- Most original exterior architectural elements intact, except has later, wide asbestos siding.
- Beautiful original interior woodwork, built-ins, and art glass windows are almost entirely intact.

1977 Marshall Avenue

Albert A. & Agnes Loudon Fry house, built in 1906

Architect: Charles W. Buechner (worked for Clarence H. Johnston; notable commissions include three used today on University Avenue: Specialty Building [1908] at Raymond, Carleton Place Lofts [1909], and fire station at St. Albans Street [1908])



1977 Marshall Avenue (View No. 2)

Albert A. & Agnes Loudon Fry house, with original carriage house behind

Architect: Charles W. Buechner



1973 Marshall Avenue
Harry Metcalfe house, built in 1906
Architect: Not shown on original building permit



Analysis of Findings Required for Site Plan Review

4. *Protection of adjacent and neighboring properties through reasonable provision for such matters as surface water drainage, sound and sight buffers, preservation of views, light and air, and those aspects of design which may have substantial effects on neighboring uses.*

Surface water drainage

- Over 3,100 SF of paved, impervious surface in the east side yard located 4' of the shared eastern property line.
 - Adjacent structure (1969 Marshall Avenue) to the east is located less than 2' from the shared property line.
 - The driveway slopes 1.5% towards the east and there is only a 4' wide buffer. This will undoubtedly cause off-site surface water drainage directly on to the adjacent property during storm events and snow melt, ensuring flooding of the property and basement of 1969 Marshall Avenue.

Sound and site buffers

- The proposed garages on the east wall will impact the adjacent properties with additional car noises and lights in a side yard. There is a 4'6" planned screen which will not be sufficient given the possible traffic and additional parking likely to occur on the driveway.
- The proposed balconies are a nuisance on the west elevation and the increased height of the structure over all others in the area cannot be mitigated. The upper balcony is 275 SF, which has an occupant allowance of up to 18 people, which will generate excessive noise and encourage large gatherings. The sounds project from the higher elevation for a greater distance. Additionally, gatherings on the balconies are likely to occur in hours when the on-site management office is closed, which will impact City resources as necessary to respond to nuisance complaints.

Analysis of Findings Required for Site Plan Review

4. *Protection of adjacent and neighboring properties through reasonable provision for such matters as surface water drainage, sound and sight buffers, preservation of views, light and air, and those aspects of design which may have substantial effects on neighboring uses (continued).*

Preservation of views, light and air:

- The developer demonstrated significantly greater shadows cast during the Union Park Land Use Committee meeting on 12/18/2017. The properties to the east and west will have full shadows cast on yards and houses, before and after midday. The proposed structure will be located on the north side of Marshall Avenue, casting shadows on neighboring properties in excess of 100' throughout the year. Alternatively, the existing character of the typical Marshall Avenue three-story building does not have these same impacts.
- There are no 5-story buildings located on Marshall Avenue, between Dale Street and Cretin Avenue. This building will be visible for several blocks in all directions.
- The proposed structure will exceed 56' from the public sidewalk and will exceed the heights of all other dwellings in the area by over 20'/2 stories.
- The proposed parking area on the east side of the property will negatively impact air quality due to car exhaust within 6' of windows on the adjacent property.

Substantial effects on neighboring land uses

- The property owner has indicated to the Union Park Land Use Committee (on 10/30/2017 and 12/18/2017) he will not assign parking to each of the proposed dwellings. Instead, parking will be charged separately and will be available on a 'first-come, first-served' basis. This will inherently lead to greater demand for on-street parking, where parking restrictions are in place near 4 Seasons A+ Elementary and St. Marks schools. Additionally, there is higher on-street parking demand due to religious institutions at Moore and Iglehart and Moore and Dayton Avenues.
- The on-site management office is intended for all of MCR Property Holdings LLC, and will expand the use of the property for noncompliant commercial purposes and increase traffic to the property.

Analysis of Findings Required for Site Plan Review

5. *The arrangement of buildings, uses and facilities of the proposed development in order to assure abutting property and/or its occupants will not be unreasonably affected.*

1969 Marshall Avenue

- The impacts of the proposed structure will reasonably affect the property owner and occupants of this property due to excessive stormwater runoff, noises, exhaust and lights from cars and dwellings and the visual impact and loss of views from the front and west sides of the property. See analysis in Finding 4 for additional information.
- The proposed driveway will undoubtedly be used for additional, outdoor parking. All of the parking on the east side of the building will be accessed via the public alley.
- The shadows cast on the entirety of the structure and yard occur at midday through sunset, all-year, as demonstrated by the shadow studies provided by the Applicant.
- The on-site management office is intended for all of MCR Property Holdings LLC, and will expand the use of the property for noncompliant commercial purposes and increase traffic to the property.

1972 and 1980 Iglehart Avenue and 1985 Marshall Avenue

- The impacts to light and air exceed a reasonable amount afforded to properties in the City of St. Paul. The mass of the structure will be fortress-like and over-bearing on the single- and two-family dwellings and their rear yards. There are no 5-stories along Marshall Avenue, between Dale Street and Cretin Avenue.
- The proposed drive aisles will inherently be used for additional, outdoor parking. All of the parking on the east side of the building will be accessed via the public alley introducing significant traffic impacts on a residential alley.
- The proposed structure will be located on the north side of Marshall Avenue, casting shadows in excess of 100' throughout the year.
- The proposed balconies are a nuisance on the west elevation and there are no mitigating strategies proposed by the Applicant.
- The on-site management office is intended for all of MCR Property Holdings, LLC, and will expand the use of the property for noncompliant commercial purposes and increase traffic to the property.

Analysis of Findings Required for Site Plan Review

6. *Creation of energy-conserving design through landscaping and location, orientation and elevation of structures.*

The applicant has failed to provide energy conserving design, including but not limited to, solar energy systems, geothermal heating, passive or highly insulated building envelopes.

7. *Safety and convenience of both vehicular and pedestrian traffic both within the site and in relation to access streets, including traffic circulation features, the locations and design of entrances and exits and parking areas within the site.*

- The property owner has indicated to the Union Park Land Use Committee on 10/30/2017 and 12/18/2017 that they will not assign parking to each of the proposed dwellings. Instead, parking will be charged separately and will be available on a 'first-come, first-served' basis.
 - This will inherently lead to greater demand for on-street parking as there is no assurances any of the occupants will use the enclosed parking structures if there is no fee to park on-street.
 - The immediate area does not have City permit parking in place.
 - Parking restrictions are in place near 4 Seasons A+ Elementary and St. Mark's schools, and at the intersection of Marshall and Moore for a city bus stop.
 - There are two nearby religious institutions (Evangelical Formosan Church and the Church of St. Mark), with either no or limited off-street parking available.
- On-site bicycle parking/storage is insufficient for 61 intended occupants. The plan shows 4 bike spaces.
- A Travel Demand Management Plan has not been provided, giving no means to analyze the actual demand for parking by residents, guests, on-site management and traffic created by the on-site management office.

Analysis of Findings Required for Site Plan Review

8. *The satisfactory availability and capacity of storm and sanitary sewers, including solutions to any drainage problems in the area of the development.*

The Project was reviewed at a site plan review meeting on Tuesday, November 7, 2017. The project has been significantly modified and updated since then, as required by Public Works, which impedes the analysis for this finding.

9. *Sufficient landscaping, fences, walls and parking necessary to meet the above objectives.*

- There is insufficient landscaping proposed to mitigate stormwater runoff to the east.
- The proposed landscape buffer is insufficient to provide a reasonable screen to adjacent properties from the proposed on-site parking area and driveways.
- See Exhibit E for parking analysis. Parking is insufficient as proposed by the applicant without approval of major variances.

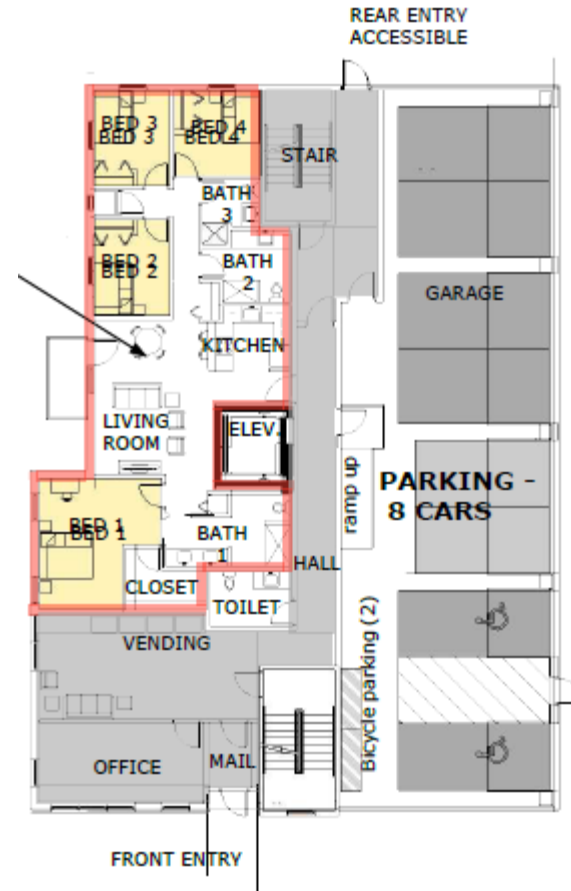
Analysis of Findings Required for Site Plan Review

10. *Site accessibility in accordance with the provisions of the Americans with Disabilities Act (ADA), including parking spaces, passenger loading zones and accessible routes.*

The public entrance is located on the south side of the proposed structure, facing Marshall Avenue. This entrance is not accessible from the public street or sidewalk. The proposed accessible entrance is at the rear of the building. **The project does not comply with Section 1105 Accessible Entrances of the 2015 MN Accessibility Code, because less than 60% of the entrances are accessible.**

Section 1105 Accessible Entrances

1105.1 Public entrances. In addition to *accessible* entrances required by Sections 1105.1.1 to 1105.1.5, at least 60 percent of all *public entrances* to each building, facility and tenant space shall be *accessible*.



Analysis of Findings Required for Site Plan Review

10. *Site accessibility in accordance with the provisions of the Americans with Disabilities Act (ADA), including parking spaces, passenger loading zones and accessible routes (continued).*

There are two parking stalls proposed in the parking garages along the east elevation. These accessible parking stalls are required to have a height clearance of 8' 2". The proposed parking garage doors are shown to be less than 8' in height, and therefore, do not comply with the Accessible Parking Section 1106 of the 2015 MN Accessibility Code.

Additionally, the accessible open parking space at the rear appears to have a slope greater than 1:48 (2%) and therefore does not comply with the Accessible Parking Section 1106 of the 2015 MN Accessibility Code.



11. *Provision for erosion and sediment control as specified in the "Ramsey Erosion Sediment and Control Handbook".*

No comment

From: pcc322 <pcc322@centurylink.net>

Sent: Wednesday, December 27, 2017 9:21:53 PM

To: Englund, Cherie (CI-StPaul); Diatta, YaYa (CI-StPaul); Johnson, Tony (CI-StPaul)

Subject: Merriam Park development.

To the Zoning Committee:

I am an active member of the congregation of the Evangelical Formosan Church at 1982 Iglehart.

I am concerned about the plan to tear down the house at the NE corner of Marshal and Moore.

I arrive early on Sunday before most of the congregation, and at that early hour, I can't always find a convenient place to park. Those who arrive later may have to walk nearly a block.

We have a couple of handicapped members, and though there isn't any designated handicapped parking places, most of us try to leave a convenient spot or two.

I know from a brief encounter that the tight parking situation on Sunday mornings does irritate some of the neighbors, and I sympathize with them.

So... As it is there is barely enough parking in the neighborhood as it is. Adding more automobiles to the on street parking will cause problems for both the residents in the neighborhood and those who want to park on the street just once or twice a week.

I urge you to withhold approval of the development plans for that neighborhood.

James A Willmore
18795 Partridge Circle
Eden Prairie. MN 55346

From: Meg Arnosti <arnosti.meg@gmail.com>

Sent: Wednesday, December 27, 2017 11:54:18 PM

To: Englund, Cherie (CI-StPaul); Diatta, YaYa (CI-StPaul); Johnson, Tony (CI-StPaul); dan.edgerton@stantec.com; blindeke@gmail.com; cedrick.baker@gmail.com; adejoy@esndc.org; christopher.james.ochs@gmail.com; ecr@trios-llc.com

Subject: Oppose 1973 and 1977 Marshall Avenue development

Dear Mr. Edgerton and Members of the Zoning Committee:

I urge you to oppose the development at 1973/1977 Marshall Avenue for the following reasons.

Union Park's 10-Year Community Plan calls for preserving the traditional feel of the neighborhood, discouraging multi-unit housing that is incompatible with single-family houses, and integrating historic significance into housing decisions. The existing homes at 1973 and 1977 Marshall contribute to the historic and unique character of Marshall Avenue, but the proposed apartment building would be destructive to that character.

The City Council approved a one-year development moratorium along these six-blocks of Marshall Avenue specifically to give a chance to evaluate the destruction of this sort of historic housing. On that very day, the developer squeaked in under the wire and submitted his hasty proposal to try to avoid being subject to the moratorium.

The University of St. Thomas has promised for years to supply enough student housing for its students and has failed to do so. This results in pressure to destroy the beautiful residential neighborhood forever.

The west elevation of the building as proposed exceeds the maximum allowed height of 50 feet because it is elevated on a hill from the street.

Marshall Avenue is a medium-density residential corridor in the City's Comprehensive Plan. This allows a maximum density of 30 units per acre; the density of the proposed project is 49 units per acre.

Only 32 parking spaces are proposed for 64 residents, and tenants will have to pay extra for these spots. This means that parking will overflow into already-crowded residential streets.

This development would unreasonably burden the neighboring homes, especially 1969 Marshall Avenue, and also 1985 Marshall Avenue and 1972 and 1980 Iglehart Avenue, with stormwater runoff, car noise and fumes.

Please keep in mind that once the character of Marshall Avenue changes, the charm of one of our great streets will be lost forever. Thank you for your careful consideration of this matter.

Sincerely,
Margaret Malde-Arnosti
1722 Princeton Avenue
St. Paul, MN 55105

From: Helen Holtti <hholtti@yahoo.com>
Sent: Thursday, December 28, 2017 9:58 AM
Subject: Moore and Marshall apartments
To: Englund, Cherie (CI-StPaul) <cherie.englund@ci.stpaul.mn.us>

Cherie,

I am writing to express my opposition to the apartment on Marshall and Moore. My daughter lives at 1999 Iglehart- just around the corner. I am a senior citizen and come over to visit my daughter often. On her side of the block is 11+ students living on the odd side of the block. Parking near her home is already practically impossible. If 50+ more cars are added - I will be carrying groceries for blocks!!! The students aren't aware of not blocking crosswalks or leaving a space in front even though they have been asked to do so. Plus 14+ busses go down Moore and Iglehart every school day making the streets very busy and congested. If a bus is traveling down the street you have to wait for it to pass - there is only room for one vehicle at a time because there are many cars on both sides of the street.

Please pass my email on to all members

Helen Holtti
651-280-7509

Sent from my iPhone

From: Madison Pierce <madison27pierce@gmail.com>

Sent: Thursday, December 28, 2017 11:41:34 AM

Subject: Marshall & Moore Apartment Development

Hello,

I'm a resident at 1990 Marshall avenue (across the street from the proposed development) and I oppose this project. While increased density in the neighborhood is essential, the units are simply unaffordable. We don't need to add to the luxury apartment bubble we currently see in the Twin Cities. It's not hard to see why \$800/bedroom/month is unrealistic for most people.

I would support the project if the units were affordable. I don't even care what they look like! I'm not annoyed by students being around the neighborhood! I'm not concerned about noise or parking and I don't even mind living across the street from a development project. I support density but I cannot support the construction of more luxury apartments.

Madison Pierce
1990 Marshall Avenue

From: Emma Kopp <em.kopp94@gmail.com>

Sent: Thursday, December 28, 2017 11:53:28 AM

To: Englund, Cherie (CI-StPaul); Diatta, YaYa (CI-StPaul); Johnson, Tony (CI-StPaul); dan.edgerton@stantec.com; blindeke@gmail.com; cedrick.baker@gmail.com; adejoy@esndc.org; christopher.james.ochs@gmail.com; ecr@trios-llc.com; Henningson, Samantha (CI-StPaul); #CI-StPaul_Ward4; #CI-StPaul_Ward7

Subject: Statement of Opposition to the Marshall & Moore Apartments

Dear Zoning Committee,

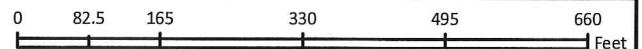
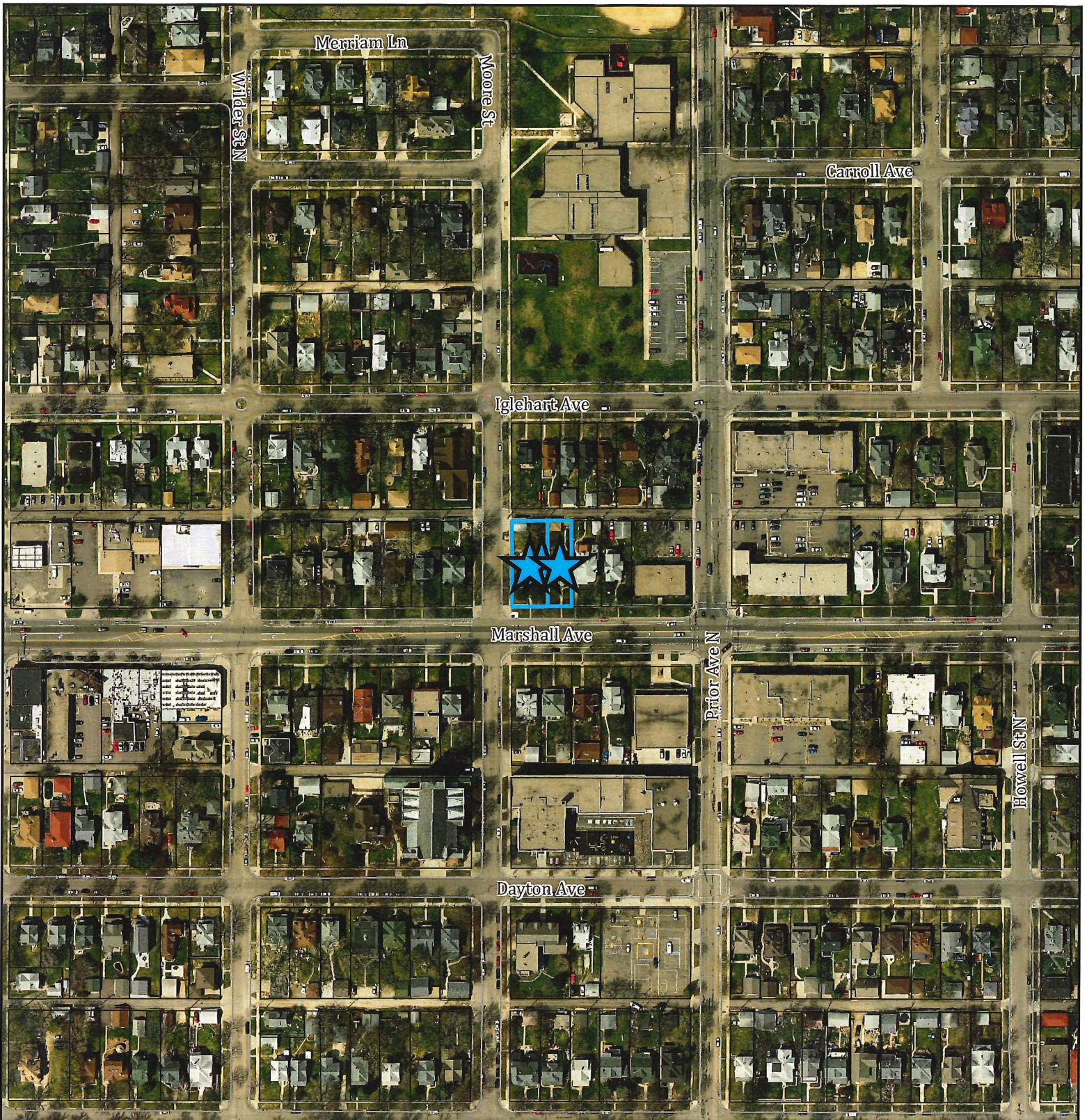
I am writing to urge you to join me and many others in opposing construction of the proposed Marshall and Moore Apartments.

First and foremost, Mr. Schwartzman's plans do not, in any way, support affordable housing. \$800 a room, for student or otherwise, is not affordable housing.

While I understand and feel the weight of a growing housing crisis in the Twin Cities, and namely our Merriam Park community, this proposition is not the correct way to go about it. The construction of apartments like the Marshall and Moore Apartments do nothing to conserve the history and charm of our neighborhood and will lead directly to more gentrification of the area.

Developing these apartments is a disgusting corporate debasement of our community and I urge you to oppose the Marshall and Moore Apartments project.

With respect,
Emma Kopp
1696 Marshall Ave
763-412-0704



FILE NAME: Marshall & Moore Apartments

Aerial

APPLICATION TYPE: Site Plan Review

 Subject Parcels

FILE #: 17-206385 DATE: 12/22/2017

PLANNING DISTRICT: 13

ZONING PANEL: 13

Saint Paul Department of Planning and Economic Development and Ramsey County





FILE NAME: Marshall & Moore Apartments

APPLICATION TYPE: Site Plan Review

FILE #: 17-206385 DATE: 12/22/2017

PLANNING DISTRICT: 13

ZONING PANEL: 13

Land Use

- Single Family Detached
- Single Family Attached
- Multifamily
- Office
- Retail and Other Commercial
- Mixed Use Residential
- Institutional

- Park, Recreational or Preserve
- Subject Parcels
- Section Lines





FILE NAME: Marshall & Moore Apartments

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Zoning

Subject Parcels

• • Section Lines

