



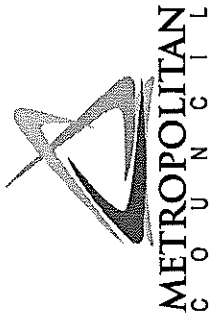
Sound Level Variance Application

Legislative Code Chapter 293. - Noise Regulations Application and \$178 fee should be submitted a minimum of sixty (60) days prior to the event date to allow ample time for required public notification period and scheduling of a Council public hearing. Applications submitted within sixty (60) days of the event date may not satisfy the processing timeline requirements.

1. Organization/person seeking variance: Metro Transit
2. Event Name: Green Line Light Rail Transit Shutdown
3. Address and physical description of noise source location (Event, Worksite): Cedar St Bridge over I-35E
4. Responsible person: Michael Wnek Title: Principal Project Coordinator
5. Telephone: 708-752-5504 E-Mail: michael.wnek@metrotransit.org
6. Date(s) variance requested: 05/14/2022 - 05/15/2022
7. Noise source - Time(s) of operation: 07:00-19:00
- Time(s) of pre-event sound check: -
8. Sound level requested (dBA/Decibels): 130
9. Mailing address w/zip code: 390 North Robert Street, St Paul, MN 55101
10. Briefly describe the noise source and equipment involved: Construction equipment including jackhammers, concrete saw, basic heavy equipment, concrete mixing equipment, etc.
11. Describe the steps that will be taken to minimize the noise levels: No mitigation due to construction location. Limit construction activities to the hours of 07:00-19:00.
12. State reason for seeking variance (example - music, announcements, construction, etc.): Repair/Maintenance
13. Maximum number of attendees: Construction Workers
14. A site diagram & map must be attached showing location of noise source(s), streets, stages, tents, etc. (If there will be amplified sound, indicate location and direction that all speakers will be facing. Multiple locations may require more than one application.)
15. Submit completed application, site diagram/map, and \$178 fee to:
CITY OF SAINT PAUL, DEPARTMENT OF SAFETY AND INSPECTIONS
375 JACKSON STREET, SUITE 220
SAINT PAUL, MN 55101-1806

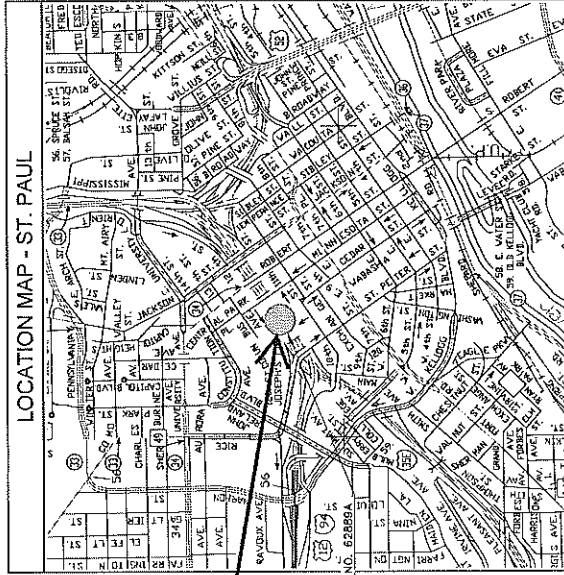
I understand any social gathering associated with this variance must be managed in compliance with any applicable Mayor Carter executive order regarding vaccinations, distancing, masks and attendance limits.

Signature of responsible person: Michael A Wnek Date: 04-18-2022



LRT GUIDEWAY SPALLING & CRACKING REPAIR: CEDAR ST BRIDGE IN ST. PAUL

PROJECT NO. 42200
CONTRACT NO. XXPXXX
ST. PAUL, MN
APRIL 2022



Location of Noise on
Cedar St Bridge over
I-35E/I-94 between
11th & 12th Streets.

SHEET INDEX	
G1.00	COVER SHEET
G1.01	CONSTRUCTION NOTES
S1.01	TYPICAL SECTION
S1.01	REPAIR IMAGES
S2.01	

PROJECT TEAM	
OWNER	METRO TRANSIT
	850 SIXTH AVENUE NORTH
	MINNEAPOLIS, MN 55411-4296
	PHONE: (612) 349-7824
CIVIL ENGINEER	
	767 EUSTIS STREET, SUITE 100
	ST. PAUL, MN 55114-2006
	PHONE: (651) 845-4197

NO. DATE DESCRIPTION OF REVISIONS 0 04/09/2022 50% Gantt Review		DESIGNED BY: MSK DRAWN BY: LMT CHECKED BY: BLF DATE: 04/09/2022	I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR CONTRACT DOCUMENTS WERE PREPARED, REVIEWED, SUPERVISED AND THAT I AM A FULLY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. DATE: 04/09/2022, BY LIC. NO.: 49488E	Kimley»Horn 767 EUSTIS STREET SUITE 100 ST. PAUL, MINNESOTA 55114 P: 612-845-4197 www.kimley-horn.com	METROPOLITAN C O U N C I L	LRT GUIDEWAY SPALLING & CRACKING REPAIR: CEDAR ST BRIDGE IN ST. PAUL COVER SHEET	COMM. NO. 16033132 SHEET NO. G1.00
--	--	--	---	---	-------------------------------	--	---

LEGEND

- EXISTING STORM WH
- EXISTING STORM CB
- EXISTING SANITARY MH
- EXISTING LIGHT POLE
- EXISTING COMMUNICATIONS VAULT
- EXISTING ELECTRICAL HANDHOLE
- EXISTING SIGNAL CABINET
- EXISTING PEDESTAL POLE
- EXISTING WATER VALVE
- EXISTING FIRE HYDRANT
- EXISTING PROPOSED SIGN
- EXISTING PARKING METER POST
- EXISTING PAY STATION
- EXISTING BOLLARD
- EXISTING STORM SEWER
- EXISTING UNDERGROUND POWER
- EXISTING OVERHEAD POWER
- EXISTING GAS LINE
- EXISTING WATER LINE
- EXISTING SANITARY SEWER LINE

GENERAL NOTES

1. SIDEWALK AND CURB DAMAGED OR BROKEN BY CONTRACTOR OUTSIDE THE LIMITS OF CONSTRUCTION SHALL BE REPLACED IN KIND AT THE CONTRACTOR'S EXPENSE.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND UTILITIES INFORMATION PRIOR TO BEGINNING CONSTRUCTION. CONTACT Gopher State ONE CALL AT 857-454-0002 IN THE METRO AREA, OR 1-800-252-1188 TOLL FREE FOR LOCATIONS OF EXISTING UTILITIES AT LEAST 48 WORKING HOURS BEFORE BEGINNING ANY CONSTRUCTION IN ACCORDANCE WITH MINNESOTA STATUTE 21D.01, OBTAIN TICKET NUMBER AND MEET WITH REPRESENTATIVES OF THE VARIOUS UTILITIES AT THE FREE SERVICE THAT LOCATES MUNICIPAL AND UTILITY COMPANY LINES, BUT DOES NOT LOCATE PRIVATE UTILITY LINES. USE AN INDEPENDENT LOCATOR SERVICE OR OTHER MEANS IN ORDER TO OBTAIN LOCATIONS OF PRIVATE UTILITY LINES INCLUDING, BUT NOT LIMITED TO, UNDERGROUND ELECTRIC CABLES, TELEPHONE CABLES, AND CONSTRUCTION SHALL NOT BE LIMITED TO PUBLIC RIGHT-OF-WAY. ALL OTHERS SHALL BE RESPONSIBLE FOR OBTAINING ALL REQUIRED CONSTRUCTION PERMITS AND BONDS IF REQUIRED PRIOR TO CONSTRUCTION.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THAT THE PROPOSED IMPROVEMENTS SHOWN ON THIS PLAN DO NOT INTERFERE WITH ANY EXISTING UTILITIES OR STRUCTURES. THE CONTRACTOR SHALL NOTIFY THE CAR PRIOR TO INSTALLATION OF ANY PORTION OF THE WORK THAT WOULD BE AFFECTED.
4. AT ALL TIMES, THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE CONDITIONS OF THE JOB SITE, INCLUDING SAFETY OF PERSONS AND PROPERTY. THE ENGINEER'S PRESENCE OR REVIEW OF WORK DOES NOT RELIEVE THE CONTRACTOR OF THIS RESPONSIBILITY.
5. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING FULL PEDESTRIAN ACCESS TO THE PLATFORM DURING CONSTRUCTION.
6. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION STAGING OPERATIONS FOR WORK ON METRO TRANSIT GUIDEWAY AND COOPERATIVELY WORK WITH ADJACENT CONTRACTOR(S).
- 8.1. ALL COORDINATION TO BE INCIDENTAL.

CONSTRUCTION NOTES

1. WHEN TYING INTO EXISTING CURB OR GUTTER, A 5' TRANSITION SHALL BE USED TO MATCH EXISTING GEOMETRY AND GRADE.
2. NO DISPOSAL SITE IS PROVIDED. CONTRACTOR SHALL BE RESPONSIBLE FOR DISPOSAL OF ALL EXCESS MATERIAL AND DEBRIS. CONTRACTOR SHALL COMPLY WITH PROVISIONS OF MNDOT 210A.3.C.3 AND 210A.3.D.3.
3. COMPACTOR OF THE GRADING OF THE PROJECT SHALL BE BY THE "SPECIFIED DENSITY METHOD". SEE MNDOT 2105.3.F.1.
4. ALL TRAFFIC SIGNS SHALL BE INSTALLED PER CITY OF SAINT PAUL STANDARDS.

REMOVAL NOTES

1. ALL REMOVAL LIMITS SHALL BE VERIFIED IN THE FIELD BY THE COUNCIL AUTHORIZED REPRESENTATIVE (CAR) PRIOR TO REMOVAL.
2. BITUMINOUS AND CONCRETE SURFACING REMOVED BY THE CONSTRUCTION SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF OFF THE PROJECT, IN ACCORDANCE WITH MNDOT 2105.3.F.1.
3. ALL BITUMINOUS AND CONCRETE PAVEMENTS SHALL BE SAWCUT PRIOR TO REMOVAL.
4. REMOVE AND DISPOSE OF ALL BITUMINOUS PAVEMENT, CONCRETE PAVEMENT AND SIDEWALK, FOOTINGS, STRUCTURES, CURB AND GUTTER, FENCE, LANDSCAPE, UTILITIES, MANHOLES, CATCHBASINS, AND ALL OTHER MISCELLANEOUS OBJECTS WITHIN CONSTRUCTION LIMITS UNLESS LABELED OTHERWISE.

EROSION CONTROL NOTES

1. CONCRETE WASHOUT WILL NOT BE PERMITTED AT THE CONSTRUCTION SITE. TRUCKS SHALL BE STOP-CONTAINED.
2. SQUIRELS, BEAVERS AND OTHER ANIMALS SHALL NOT OCCUR IN THE CONSTRUCTION AREA.
3. THE CONTRACTOR SHALL USE MEASURES TO CONTROL DUST ON SITE, AS DIRECTED BY THE CAR.
4. PRIOR TO EARTHWORK ACTIVITIES, THE CONTRACTOR SHALL CONSTRUCT STORM DRAIN INLET PROTECTION AT LOCATIONS SHOWN ON THE PLANS.
5. CONSTRUCTION SHALL BE STOPPED AT THE END OF MATERIALS AND DEBRIS AND ALL EROSION CONTROL MEASURES SHALL BE REMOVED AT THE END OF EACH SHUTDOWN.

BASIC SPALL REPAIR SEQUENCE

1. PROTECT RAIL AND RAIL BOOT FROM OVERTIGHTING. ON ALL REPAIR AREAS.
2. SQUARE UP EDGES OF SPALLS. SPALLS SHALL BE REMOVED HORIZONTALLY TO RAIL BOOT AND NEAREST CONCRETE JOINT.
3. REMOVE ALL UNSOUND CONCRETE WITHIN NOTED REPAIR AREAS.
4. IF REINFORCEMENT IS EXPOSED, REMOVE CONCRETE ON ALL SIDES AND UNTIL AT LEAST 7" OF THE BAR SHOWS NO CORROSION.
5. COAT REINFORCEMENT AND RAIL SUPPORT DEVICES TO A "GREY" METAL CONDITION.
6. COAT EXPOSED REINFORCEMENT AND RAIL SUPPORT DEVICES WITH A ZINC-RICH EPOXY PRIMER.
7. PLACE REPAIR CONCRETE MATERIAL (INCLUDING BONDING AGENT) TO BROOM FINISH OR MATCH ORIGINAL/ADJACENT FINISH AT CONCRETE REPAIR AREAS.

ABBREVIATIONS

- ROW RIGHT-OF-WAY
- ADA AMERICANS WITH DISABILITIES ACT
- TEMP TEMPORARY
- EXIST EXISTING
- BLDG BUILDING
- TOC TOP OF CURB
- EL FLOW LINE
- EL CLEARANCE
- BT BITUMINOUS
- CL CENTERLINE
- LP LOW POINT

REMOVAL REQUIREMENTS

CONCRETE SHALL BE REMOVED BY THE CONTRACTOR WITH THE CAR PRESENT WILL IDENTIFY REMOVAL LOCATIONS BY DEFINING THE AREAS FOR REPAIR. REMOVE ONLY THAT PORTION THAT HAVE BEEN DEFINED FOR REPAIR BY THE CAR AND THE PLANS. RESTRICT REMOVAL TO METHODS WHICH, IN THE CAR'S JUDGMENT, WILL NOT DAMAGE THE GUIDEWAY.

RESTRICTIONS FOR THE POWER EQUIPMENT:

1. REPAIRS REQUIRING WHEEL POWER EQUIPMENT WHICH WAS PREVIOUSLY DEMONSTRATED TO OPERATE WITH EXCELLENCE ON THE TYPE OF WORK FOR WHICH IT IS TO BE USED, IF PERMITTED BY THE CAR, USE NEWLY DEVELOPED POWER EQUIPMENT ON A PERFORMANCE BASIS, BUT DISCONTINUE SUCH USAGE IF SO DIRECTED BY THE CAR.
2. DO NOT USE JACK-HAMMERS HEAVIER THAN A NOMINAL 30 POUND CLASS FOR REMOVAL ABOVE THE TOP LAYER OF REINFORCEMENT. EXCEPT THAT THE CAR MAY PERMIT THE USE OF UP TO A NOMINAL 60 POUND HAMMER BY INDIVIDUAL OPERATORS ON A PERFORMANCE BASIS, BUT DISCONTINUE SUCH USAGE IF THE CAR DETERMINES THAT THE HEAVIER HAMMERS ARE CREATING ADDITIONAL DELAMINATION, OR THAT THEY ARE NOT BEING USED WITH PROPER DISCRETION.
3. POINTED BITS FOR JACK-HAMMERS ARE NOT PERMITTED EXCEPT IN AREAS WHERE FULL DEPTH REMOVAL IS SPECIFICALLY DEFINED BY THE CAR.
4. DO NOT USE JACK-HAMMERS HEAVIER THAN A NOMINAL 15 POUND CLASS FOR REMOVAL BELOW THE TOP LAYER OF REINFORCING BARS UNLESS FULL DEPTH REMOVAL IS SPECIFICALLY DEFINED BY THE CAR.

ENSURE THAT THE EDGES OF ALL REMOVAL AREAS ARE NEAR VERTICAL AND CLEAN IMMEDIATELY BEFORE PLACING THE CONCRETE PATCHING MIX.

AFTER REMOVAL OPERATIONS ARE COMPLETED, CLEAN THE REMOVAL AREA OF ALL REMAINING REINFORCEMENT. REMOVE ALL SAND AND DEBRIS. REMOVE LOOSE REBAR TIGHTLY ADHERENT RUST AND MILL SCALE MAY REMAIN ON THE SURFACE REMOVE SPENT SAND AND DEBRIS.

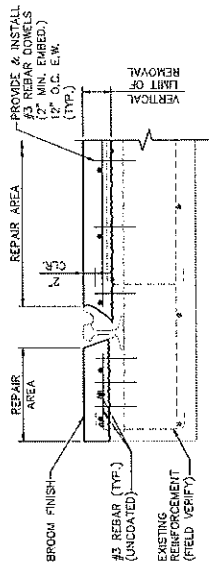
FOLLOW PROVISIONS OF MNDOT 1717. "AIR, LAND, AND WATER POLLUTION," REFERRING TO MPCA RULE 7011.050 (HITTING/WIND-EXHAUST/EMISSIONS) AS IT RELATES TO SANDBLASTING AND OR CONCRETE REMOVAL OPERATIONS.

LEAVE ALL REINFORCEMENT STEEL IN PLACE AS IT WAS BEFORE CONCRETE REMOVAL, UNLESS OTHERWISE DIRECTED BY THE CAR. REPAIR AND/OR REPLACE ALL REINFORCEMENT BARS DAMAGED BY CONTRACTOR'S OPERATIONS, AS DIRECTED BY THE CAR. ALL JOISTS INCURRED ARE CONSIDERED INCIDENTAL EXPENSES FOR WHICH NO DIRECT COMPENSATION WILL BE MADE.

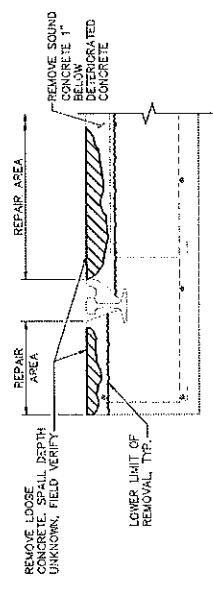
ALL DAMAGE TO OTHER PORTIONS OF THE STRUCTURE WHICH ARE TO REMAIN IN PLACE, WHICH IS DUE TO THE REMOVAL OPERATIONS WILL BE REPAIRED. ALL COSTS INCURRED ARE CONSIDERED INCIDENTAL EXPENSES FOR WHICH NO DIRECT COMPENSATION WILL BE MADE.

DO NOT PERFORM REMOVAL IN ANY AREA UNTIL THE PERIMETERS FOR REMOVAL IN THAT AREA HAVE BEEN DEFINED BY THE CAR FOR THAT TYPE OF REMOVAL.

DISPOSE OF ALL MATERIALS REMOVED IN ACCORDANCE WITH MNDOT 210A.3.C. "REMOVAL OPERATIONS".

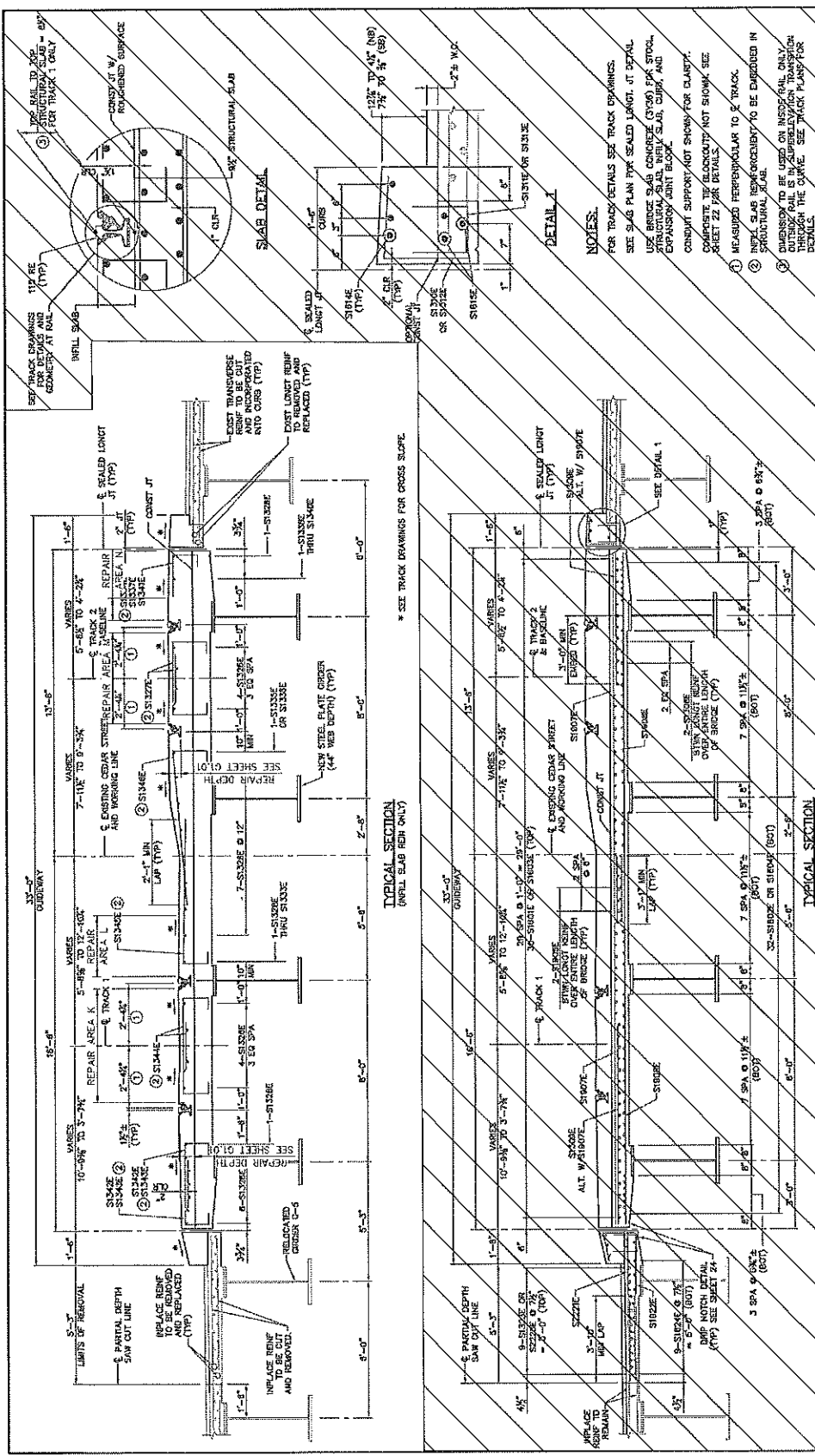


REMOVAL DETAIL



	<p>CONSTRUCTION NOTES</p> <p>SPALL REPAIR (OTHER REPAIRS SIMILAR)</p> <p>LRT GUIDEWAY SPALLING & CRACKING REPAIR: CEDAR ST BRIDGE IN ST. PAUL</p>	<p>COMM. NO. 190033102</p> <p>SHEET NO. G1.01</p>
<p>Kimley»Horn 167 SUITE 100 ST. PAUL, MINNESOTA 55114 PH: 651-447-4197 www.kimley-horn.com</p>		
<p>NO. DATE</p> <p>0 04/08/2022</p>	<p>DESCRIPTION OF REVISIONS</p> <p>APP. BY: [Signature]</p> <p>CHKD BY: [Signature]</p> <p>DATE: 04/08/2022</p>	<p>DESIGNED BY: MSK</p> <p>DRAWN BY: LMT</p> <p>CHECKED BY: BLF</p> <p>DATE: 04/08/2022</p>
<p>I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR CONTRACT DOCUMENTS WERE PREPARED BY ME OR UNDER MY SUPERVISION AND THAT I AM A duly LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.</p> <p style="text-align: right;">DATE: 04/08/2022 M.I.C. NO. 69486</p>		

LRT GUIDEWAY - REPAIR AREAS (K-N) AS-BUILT SHEET PROVIDED FOR INFORMATION ONLY, EXCEPT AS NOTED



NOTES:
 FOR TRACK DETAILS SEE TRACK DRAWINGS.
 SEE SLAB PLAN FOR SEALED JOINT DETAIL.
 USE BRIDGE SLAB CONCRETE (3000) FOR STROK.
 REINFORCEMENT SHALL BE SLAB, CURB, AND
 EXPANSION JOINT BLOCKS.
 CONJOINT SUPPORTS NOT SHOWN FOR CLARITY.
 COMPETE THE BLOCKOUTS NOT SHOWN. SEE
 SHEET 22 FOR DETAILS.
 ① MEASURED PERPENDICULAR TO TRACK.
 ② STRUCTURAL SLAB.
 ③ DIMENSIONS TO BE USED ON INSIDE RAIL ONLY.
 OUTSIDE RAIL IS IN SUPERELEVATION TRANSITION
 THROUGH THE CURVE. SEE TRACK PLANS FOR
 DETAIL.

DATE	DESCRIPTION OF REVISIONS	DESIGNED BY:	MSK
04/08/22	ISS. DRAFT REVIEW	DRAWN BY:	LAT
		CHECKED BY:	BLE
		DATE	04/08/2022
		DATE	04/08/2022

Central Corridor Light Rail Transit
 #1 Metropolitan Council

AECOM

CIVIL EAST CONSTRUCTION
 BRIDGE 62889
 SLAB TYPICAL SECTION

STRUCTURES: BR62889-STU-BRG-026

SHEET 26 OF 50

Kimley»Horn
 187 STATE STREET
 ST. PAUL, MINNESOTA 55114
 P: 651-644-1157 www.kimley-horn.com

METROPOLITAN
 C O U N C I L

COM. NO. 6003032
 SHEET NO. S0.01

LRT GUIDEWAY SPALLING & CRACKING REPAIR:
 CEDAR ST BRIDGE IN ST. PAUL

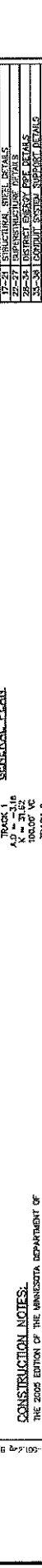
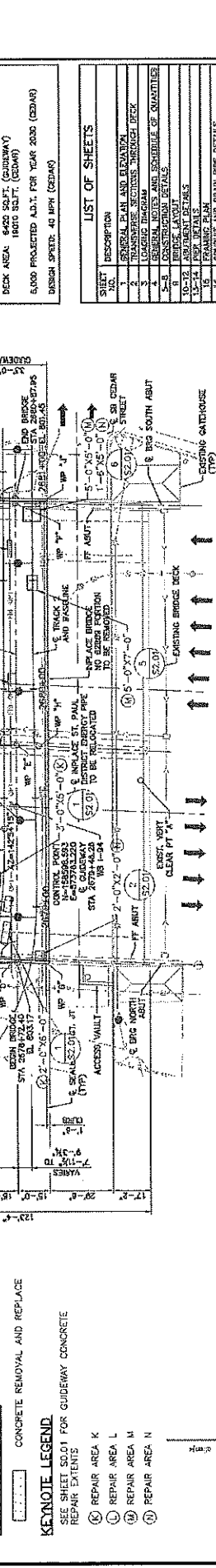
TYPICAL SECTION

NOTES:
 THE SUBSURFACE UTILITIES ARE DEPICTED AT UTILITY QUALITY LEVEL. CONTRACTOR SHALL VERIFY THE LOCATION OF EXISTING UTILITIES AND SHALL NOTIFY THE CAR OF ANY DISCREPANCIES.
 FIELD VERIFY REMOVAL LIMITS WITH CAR.
 ADJUST CONCRETE REMOVALS TO NEAREST JOINT.
 DIMENSIONS SHOWN ARE FOR INFORMATIONAL PURPOSES ONLY AND MAY VARY BASED ON FIELD VERIFICATION. AMOUNT OF REMOVALS REQUIRED TO EXPOSE REINFORCEMENT SHALL BE BASED ON FIELD VERIFICATION. ADDITIONAL QUANTITIES ASSOCIATED WITH REMOVALS SHALL BE INCIDENTAL TO THE WORK.

REMOVALS LEGEND:
 CONCRETE REMOVAL AND REPLACE
 REPAIR EXTENTS
 REPAIR AREA K
 REPAIR AREA L
 REPAIR AREA M
 REPAIR AREA N

KEYNOTE LEGEND:
 SEE SHEET SD.01 FOR GUIDEWAY CONCRETE REPAIR EXTENTS
 REPAIR AREA K
 REPAIR AREA L
 REPAIR AREA M
 REPAIR AREA N

CONSTRUCTION NOTES:
 THE 2008 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR CONSTRUCTION SHALL APPLY.
 BRIDGE SEALED JOINTS SHALL BE INSTALLED IN FINAL POSITION PRIOR TO DRILLING HOLES FOR AND PLACING ANCHOR BOLTS.
 THE TWO ENDS OF EACH BAR MARKS INDICATE THE DIAMETER OF THE BAR IN MILLIMETERS. BARS MARKED WITH THE SUFFIX "S" SHALL BE EPOXY COATED IN ACCORDANCE WITH DIVISION 1 REQUIREMENTS.
 CONSTRUCTION STAGING SHOWN IN THESE DRAWINGS IS SUBJECT TO CHANGE. CONTRACTOR SHALL VERIFY THE CONSTRUCTION STAGING APPROACH WITH THE LOCAL PERMITS OFFICE.
 STAGING IS NOT SHOWN IN THESE DRAWINGS. SEE TRAFFIC REQUIREMENTS, DETAILS AND CONSTRUCTION MANUAL FOR STAGING REQUIREMENTS.
 CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND APPROXIMATE WITH DIVISION 1 REQUIREMENTS.
 SEE OTHER DRAWINGS FOR EXISTING CONDUITS, ADJACENT REMOVALS, UTILITY LOCATIONS, UTILITY ABUTMENTS, INCLUDED IN THIS RECONSTRUCTION EXP. AT TYPE "A" AND "B" AT JENSEN TYPE SPECIAL. SEE DETAILS SHEETS 27-11.



LIST OF SHEETS

SHEET NO.	DESCRIPTION
1	GENERAL PLAN AND ELEVATION
2	TRANSVERSE SECTIONS THROUGH DECK
3	TRANSVERSE SECTIONS THROUGH DECK
4	GENERAL NOTES AND SCHEDULE OF QUANTITIES
5-8	CONSTRUCTION DETAILS
9	BRIDGE LAYOUT
10-12	ABUTMENT DETAILS
13-15	FRAMING PLAN
16	CONCRETE AND BRIDGE PIPE DETAILS
17-21	STRUCTURAL STEEL DETAILS
22-24	BRIDGE ENERGY DETAIL
25-28	LOADOUT SYSTEM SUPPORT DETAILS
29-31	EXPANSION JOINT DETAILS
32-34	BRIDGE MARKING
35-37	OVERLAY DETAILS
38-40	BRIDGE MARKING
41-43	OVERLAY DETAILS
44-46	BRIDGE MARKING
47-49	OVERLAY DETAILS
50	OVERLAY DETAILS

STATE PROJECT NO. 6280-62889B
 BRIDGE NO. 62889
 CEDAR STREET
 CENTRAL CORRIDOR LRT
 OVER I-35E IN ST. PAUL
 REGISTRATION NO. 401
 GENERAL PLAN AND ELEVATION
 SSC 34 ST. PAUL R. 32 N.
 CITY OF ST. PAUL RAMSEY COUNTY
 APPROVED: STATE BRIDGE ENGINEER
 DATE:

DESIGN DATA (GUIDEWAY ONLY)
 2009 AREA MANUAL FOR RAILWAY ENGINEERING
 2009 AREA MANUAL FOR RAILWAY ENGINEERING
 DESIGN METHOD: HL-SI LOADING
 LIVE LOADING (SEE SHEET 3)
 HARMON ALLOWABLE DESIGN STRESS
 REINFORCEMENT: 60 MPA
 STRUCTURAL STEEL: A572-50
 STRUCTURAL STEEL: A572-50
 DECK AREA: 6400 SQ.FT. (GUIDEWAY)
 1600 SQ.FT. (CEM) (CEM)
 DESIGN SPEED: 40 MPH (CEDAR)

CIVIL EAST CONSTRUCTION
 BRIDGE 62889
 GENERAL PLAN AND ELEVATION
 STRUCTURES: BRG2889-STU-BRG-001
 SHEET 1 OF 50

Central Corridor Light Rail Transit
 Metropolitan Council
 AECOM
 Kimley-Horn
 METROPOLITAN

LRT GUIDEWAY SPALLING & CRACKING REPAIR:
 CEDAR ST BRIDGE IN ST. PAUL
 REPAIR PLAN
 \$1.01

NO.	DATE	DESCRIPTION OF REVISIONS	APP. BY	DESIGNED BY:	MSK	DRWN BY:	LMT	CHECKED BY:	BLF	DATE:
1	04/02/2022									04/02/2022

DATE: 04/02/2022
 DRAWING NO.: 62889
 SHEET NO.: S1.01

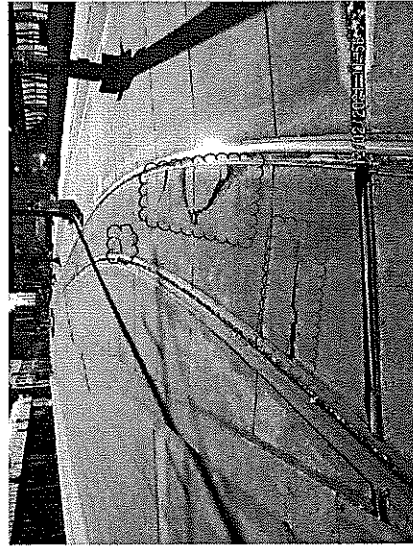


PHOTO 1 - CEDAR ST. BRIDGE OVER I-35E

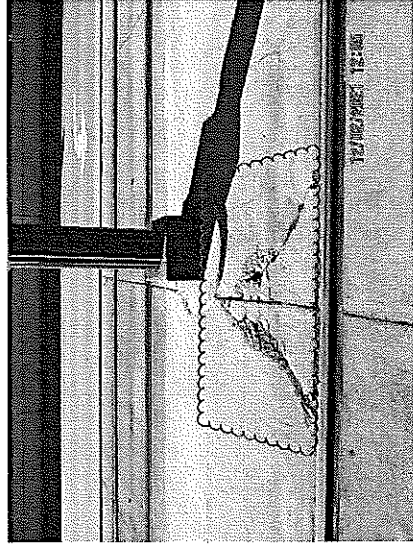


PHOTO 3 - CEDAR ST. BRIDGE OVER I-35E



PHOTO 5 - CEDAR ST. BRIDGE OVER I-35E

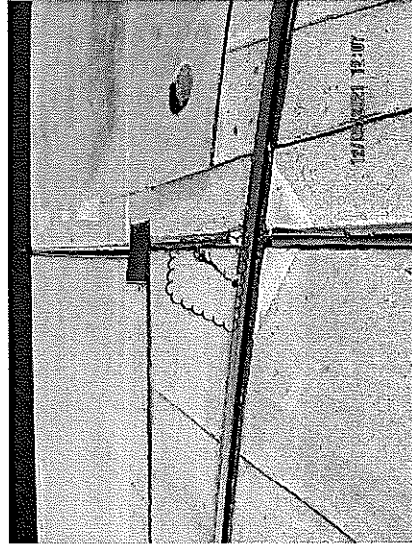


PHOTO 2 - CEDAR ST. BRIDGE OVER I-35E

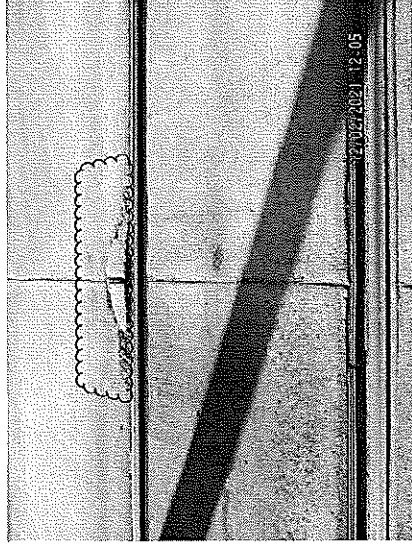


PHOTO 4 - CEDAR ST. BRIDGE OVER I-35E

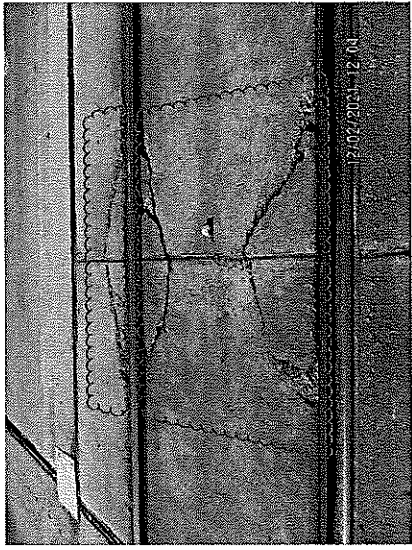


PHOTO 6 - CEDAR ST. BRIDGE OVER I-35E

NO. 10 DATE: 04/09/2022 DESCRIPTION OF REVISIONS 1. SPALL REPAIR	DESIGNED BY: MSK DRAWN BY: LMT CHECKED BY: BLF DATE: 04/09/2022	I HEREBY CERTIFY THAT THIS PLAN SHEET IS A TRUE AND CORRECT REPRESENTATION OF THE WORK AS SUPERVISED BY ME OR UNDER MY SUPERVISION AND THAT I AM A duly LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA. DATE: 04/09/2022 MINN. REG. # 6488 BRIAN F. HORN, P.E.	<p>Kimley-Horn 107 5th Street St. Paul, Minnesota 55114 Ph: 651-437-1111</p>	<p>METROPOLITAN COUNCIL</p>	LRT GUIDEWAY SPALLING & CRACKING REPAIR: CEDAR ST BRIDGE IN ST. PAUL REPAIR IMAGES S2.01	COMM. NO. 06820132 SHEET NO.
---	--	---	--	------------------------------------	--	---------------------------------



DSI RECEIPT

CITY OF SAINT PAUL
Department of Safety and Inspections
375 Jackson Street Suite 220
Saint Paul, Minnesota 55101-1806
Phone: (651) 266-8989 Fax: (651) 266-9124
www.stpaul.gov/dsi

Date: 04/22/2022

Received From: JULIE BRENNY dba: METRO TRANSIT
390 ROBERT ST PAUL MN 55101

Description:

Invoice Details

1125153

Noise Variance

Invoice Amount

\$178.00

Amount Paid

\$178.00

TOTAL AMOUNT PAID:

\$178.00

Paid By:

Payment Type	Check #	Received Date	Amount
Credit Card	V0195	04/22/2022	\$178.00